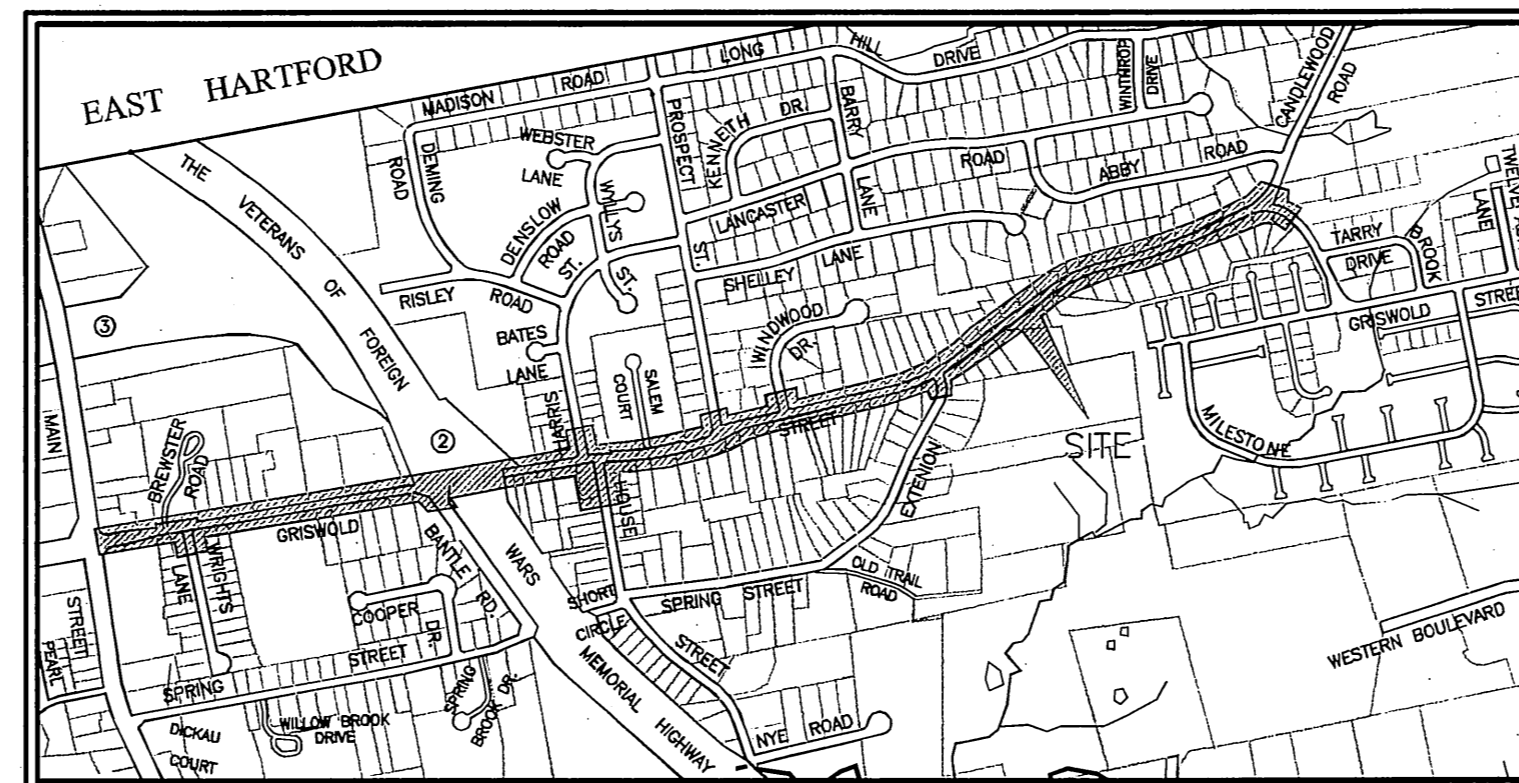


TOWN OF GLASTONBURY ENGINEERING DEPARTMENT GRISWOLD STREET REHABILITATION STATE PROJECT No. 53-184 PW-1014 located between MAIN STREET & CANDLEWOOD ROAD GLASTONBURY, CONNECTICUT

SHEET INDEX

SHEET NO.	DESCRIPTION
1	TITLE SHEET
2	DETAIL ESTIMATE SHEET
3,4	TYPICAL CROSS SECTIONS TYPICAL DETAILS
5 - 10	PLAN & PROFILES
11,12	PAVEMENT MARKING PLANS
13-15	CROSS-SECTIONS
TR-1010_01	CONCRETE HANDHOLE
TR-1111_01	LOOP VEHICLE DETECTOR AND SAWCUT
TR-1208_02	METAL SIGN POST AND SIGN MOUNTING DETAILS
TR-1210_03	SPECIAL DETAILS AND PAVEMENT MARKINGS FOR TWO-WAY HIGHWAYS
TR-1220_01	SIGNS FOR CONSTRUCTION AND PERMIT OPERATIONS
TR-1220_02	CONSTRUCTION SIGNS SUPPORTS AND CHANNELIZING DEVICES



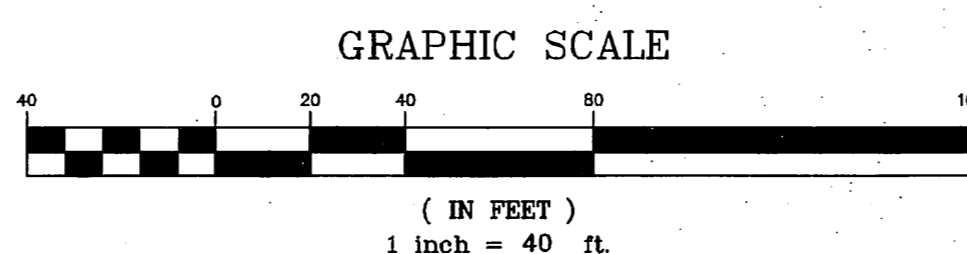
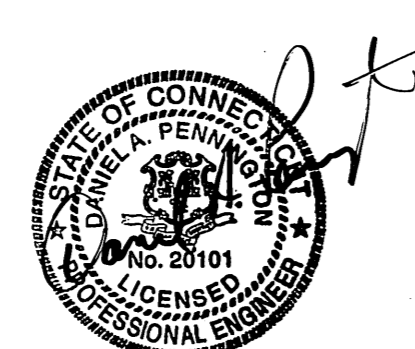
LOCATION MAP
SCALE: 1"=1000'

NOVEMBER 2011

ISSUED FOR CONSTRUCTION

RICHARD J. JOHNSON
TOWN MANAGER

DANIEL A. PENNINGTON
MANAGER OF PHYSICAL SERVICES/TOWN ENGINEER

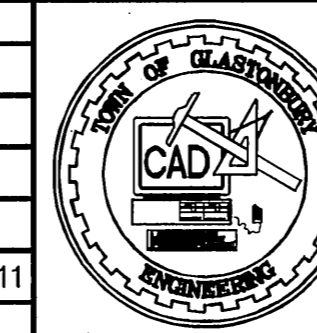


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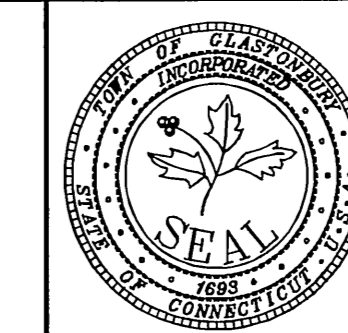
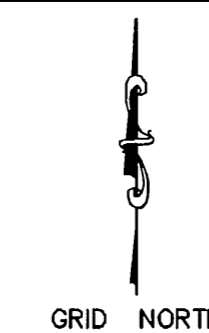
MANUAL REVISIONS TO THIS DOCUMENT ARE PROHIBITED.
ALL REVISIONS MUST BE PERFORMED ON CADD FILE.
H:\DWG\Streets\Griswold St\PW1014-Griswold St Pavement Rehab\Griswold St Pavement Rehab - 10-2011.dwg

STP 53-184
PW-1014

DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011



SCALE: AS SHOWN	DATE:
DRAWN BY: C.F.S.	11/11/2010
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	
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TITLE SHEET
PAVEMENT REHABILITATION
FOR
GRISWOLD STREET
LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

SHEET NO.
1
OF 15

FILE: F:\DWG\Estimate\Griswold\80711014-Griswold-St-Pavement-Rehab-10-2011.dwg USBC: Claudia Schmalzer DATE: 1/28/2011

THE INFORMATION, INCLUDING ESTIMATED QUANTITIES OF WORK, SHOWN ON THESE SHEETS IS BASED ON LIMITED INVESTIGATIONS BY THE STATE AND IS IN NO WAY WARRANTED TO INDICATE THE TRUE CONDITIONS OR ACTUAL QUANTITIES OR DISTRIBUTION OF QUANTITIES OF WORK WHICH WILL BE REQUIRED

DETAILED ESTIMATE SHEET

P = FEDERAL AID PARTICIPATING
 NP = FEDERAL AID NON PARTICIPATING
 *FOR INTERNAL USE ONLY
 NOT A LEGAL DEFINITION

FOR THE CONSTRUCTION OF GRISWOLD STREET REHABILITATION

IN THE TOWN OF GLASTONBURY

FROM STA. 4+75 TO STA. 68+50 LENGTH 6,375 ft; BASE COURSE VARIES WIDTH 27 ft; DEPTH 0.5 ft SURFACE COURSE SUPERPAVE 0.375" WIDTH VARIES ft; DEPTH -- in;

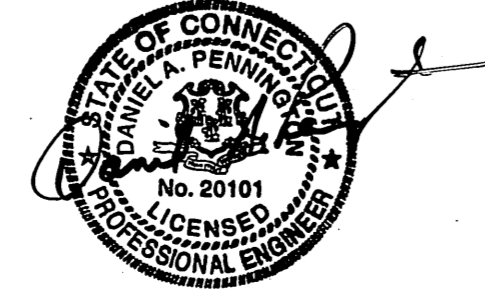
ITEM NUMBER	ITEM										TRENCH										EXCAVATION									
	SUPERPAVE 0.500"	SUPERPAVE 0.375"	SUPERPAVE 0.250"	PROCESSED STONE BASE	MATERIAL FOR TACK COAT	FINE MILLING (0-4")	EARTH EXCAVATION	CUT BITUMINOUS CONCRETE PAVEMENT	FORMATION OF SUBGRADE	SUBBASE	PEDESTRIAN RAMP	CONC. SIDEWALK	PARTIAL DEPTH PAVEMENT PATCH	PERMANENT DRIVEWAY REPAIRS	BITUMINOUS CONC. LIP CURB (6")	BITUMINOUS CONC. PARK CURB (4")	RESET GRANITE STONE CURBING	RESET BRICK PAVERS	SEDIMENT CONTROL SACK	CALCIUM CHLORIDE FOR DUST CONTROL	WATER FOR DUST CONTROL	GRADING & TOPSOIL	TURF ESTABLISHMENT	SODDING	PREPARATION OF SITE	MAINTENANCE AND PROTECTION OF TRAFFIC	TRAFFIC PERSON (UNIFORMED FLAGGER)			
UNIT	TON	TON	TON	C.Y.	GAL.	S.Y.	C.Y.	L.F.	S.Y.	C.Y.	EA.	S.F.	S.Y.	S.Y.	L.F.	L.F.	L.F.	S.F.	EA.	TON	M.GAL.	S.Y.	S.Y.	S.Y.	L.S.	L.S.	HR.			
GRISWOLD STREET	850	2,550	360	20	2,100	17,700	1,600	200	3,000	900	21	2,800	400	630	7,700	2,800	200	50	6	1.5	50	2,900	2,900	50	L.S.	L.S.	480			
SUBTOTAL	850	2,550	360	20	2,100	17,700	1,600	200	3,000	900	21	2,800	400	630	7,700	2,800	200	50	6	1.5	50	2,900	2,900	50	L.S.	L.S.	480			
UNASSIGNED																														
TOTALS	850	2,550	360	20	2,100	17,700	1,600	200	3,000	900	21	2,800	400	630	7,700	2,800	200	50	6	1.5	50	2,900	2,900	50	L.S.	L.S.	480			

ITEM	15" RCP CLASS IV	TYPE "C" CATCH BASIN	REPLACE TYPE "C" CATCH BASIN TOP	CONVERT CATCH BASIN TO TYPE C CATCH BASIN	CONVERT CATCH BASIN TO MANHOLE	RESET MANHOLE	RESET WATER GATE	RESET GAS GATE	SIGN FACE SHEET (MINIMUM TYPE III REFLECTIVE SHEETING)	CONSTRUCTION SIGNS TYPE III REFLECTIVE SHEETING	TRAFFIC CONE	TRAFFIC DRUM	CONSTRUCTION BARRICADE TYPE III	OPPOSING TRAFFIC LANE DIVIDER	BARRICADE WARNING LIGHTS HIGH INTENSITY
	L.F.	EA.	EA.	EA.	EA.	EA.	EA.	EA.	S.F.	S.F.	EA.	EA.	EA.	EA.	DAY
GRISWOLD STREET	60	3	33	4	3	5	12	12	15	250	40	40	12	8	1,200
SUBTOTAL	60	3	33	4	3	5	12	12	15	250	40	40	12	8	1,200
UNASSIGNED															
TOTALS	60	3	33	4	3	5	12	12	15	250	40	40	12	8	1,200

ITEM	CONCRETE HANDHOLE TYPE II EXTENSION	CONCRETE HANDHOLE TYPE II	CAST IRON HANDHOLE COVER TYPE II	LOOP DETECTOR SAWCUT	TEMP MARKINGS	PAVEMENT MARKINGS			FEDERAL NON-PARTICIPATING ITEMS	ALTERNATE ITEMS
	EA.	EA.	EA.	L.F.	4" YELLOW PAINTED PAVEMENT MARKING	4" WHITE EPOXY RESIN PAVEMENT MARKING	4" YELLOW EPOXY RESIN PAVEMENT MARKING	EPOXY RESIN PAVEMENT MARKING SYMBOLS & LEGENDS		
GRISWOLD STREET	1	1	1	75	5,000	200	12,000	3,075		
SUBTOTAL	1	1	1	75	5,000	200	12,000	3,075		
UNASSIGNED										
TOTALS	1	1	1	75	5,000	200	12,000	3,075		

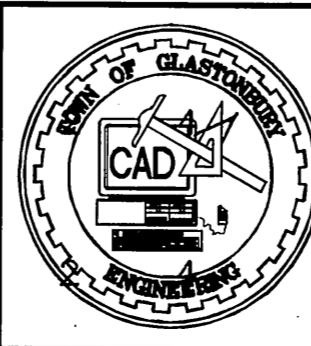
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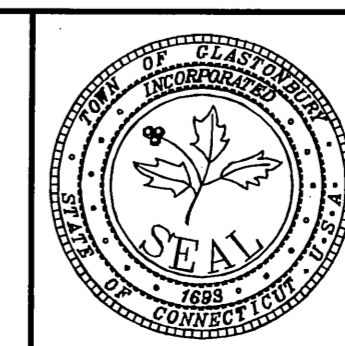
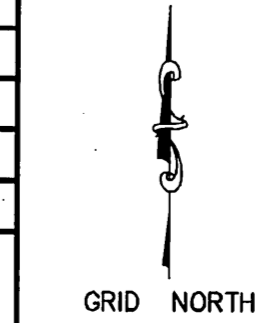


STP 53-184
PW-1014

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	DATE: 11/23/2011

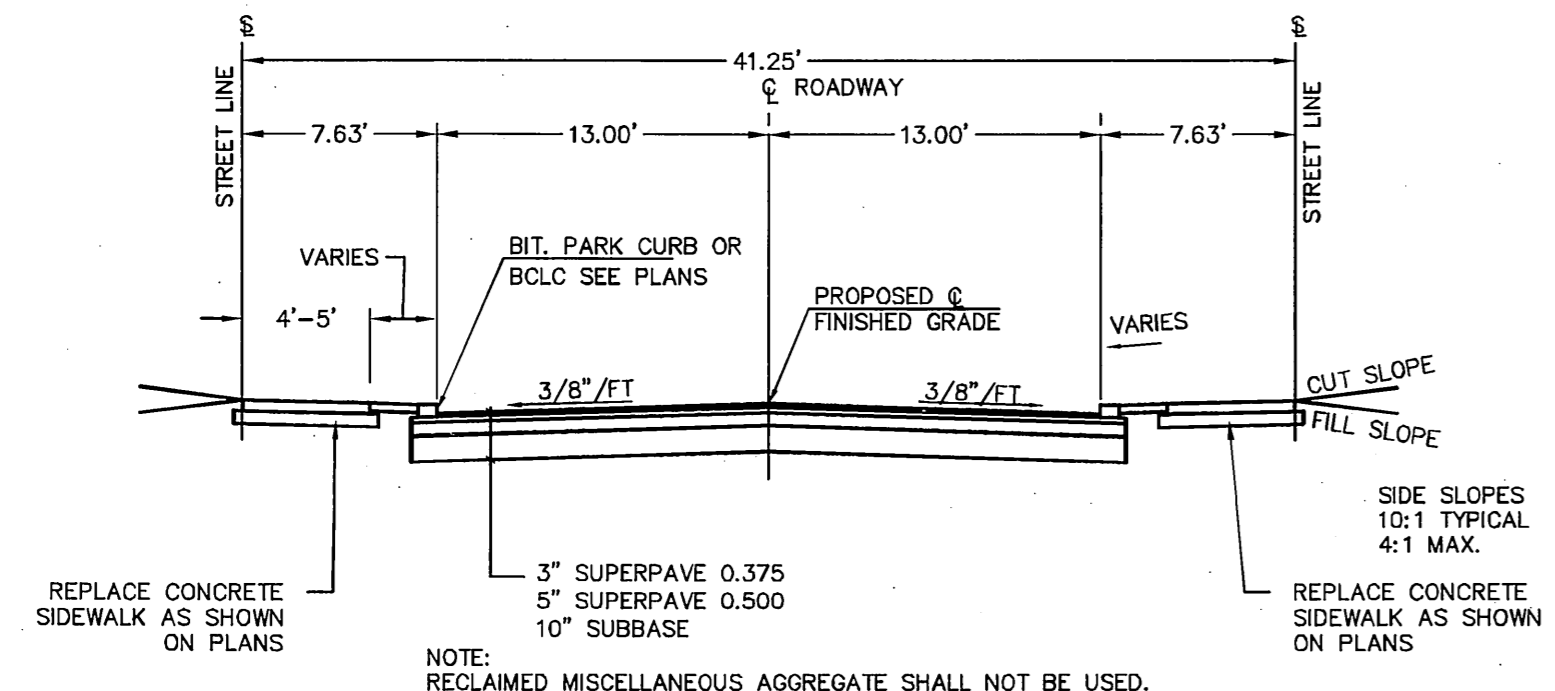


SCALE: AS SHOWN	DATE:
DRAWN BY: C.F.S.	1/11/2011
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	

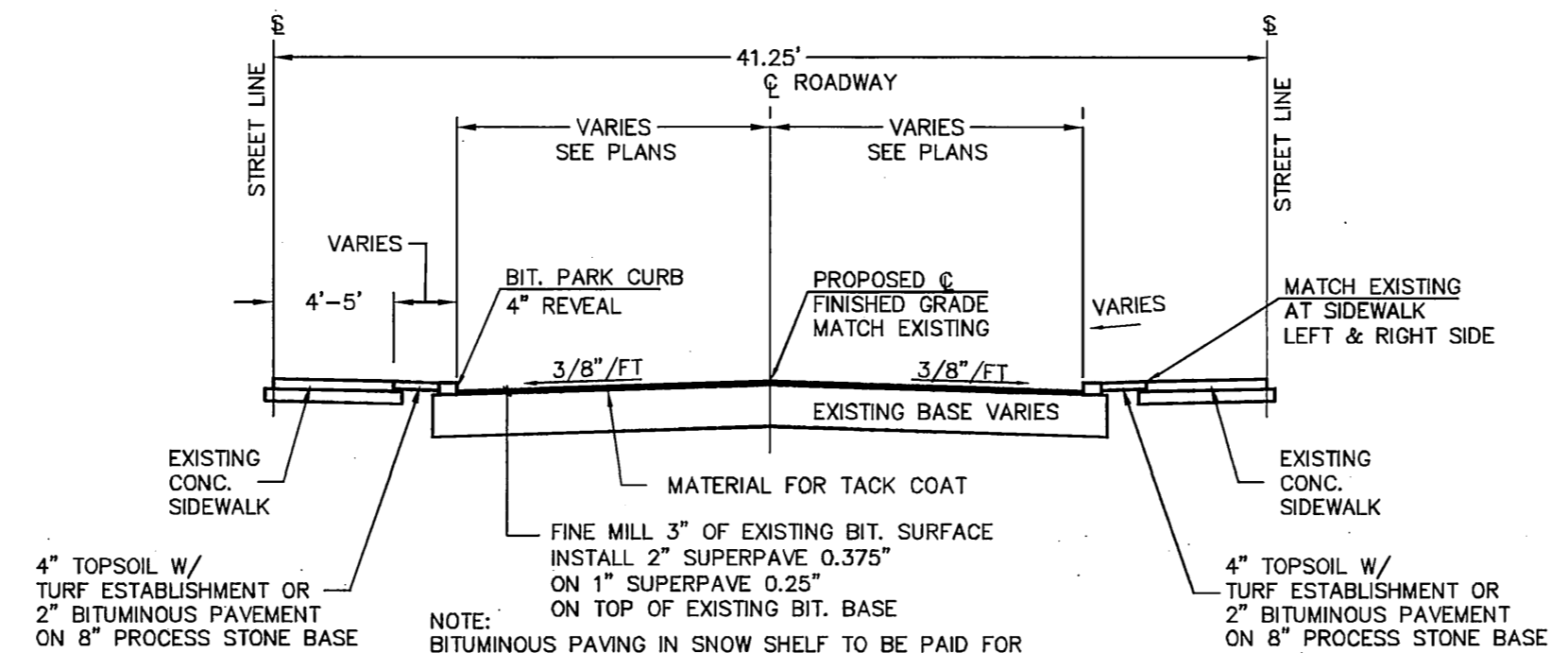


DETAILED - ESTIMATE SHEET
 PAVEMENT REHABILITATION
 FOR
 GRISWOLD STREET
 LOCATED BETWEEN
 MAIN STREET & CANDLEWOOD ROAD
 GLASTONBURY, CONNECTICUT

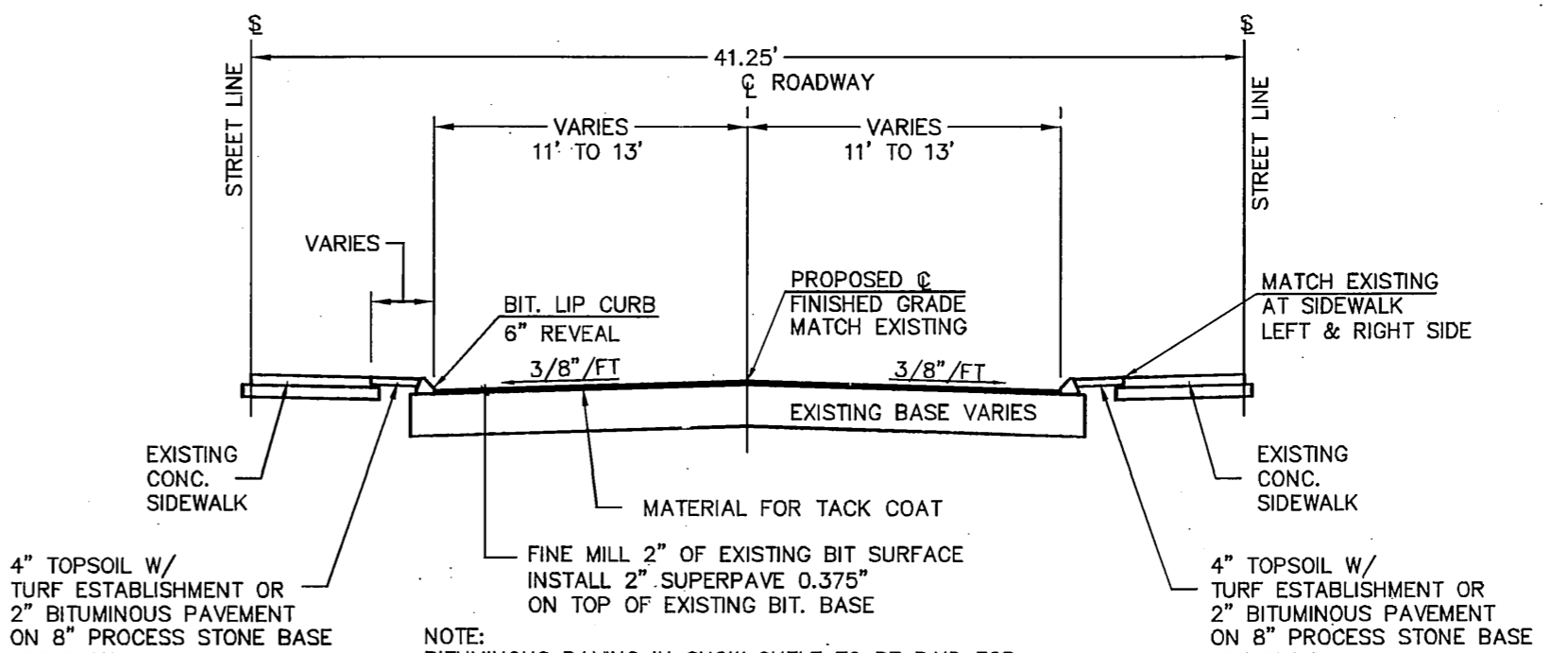
SHEET NO. **2**
OF 15



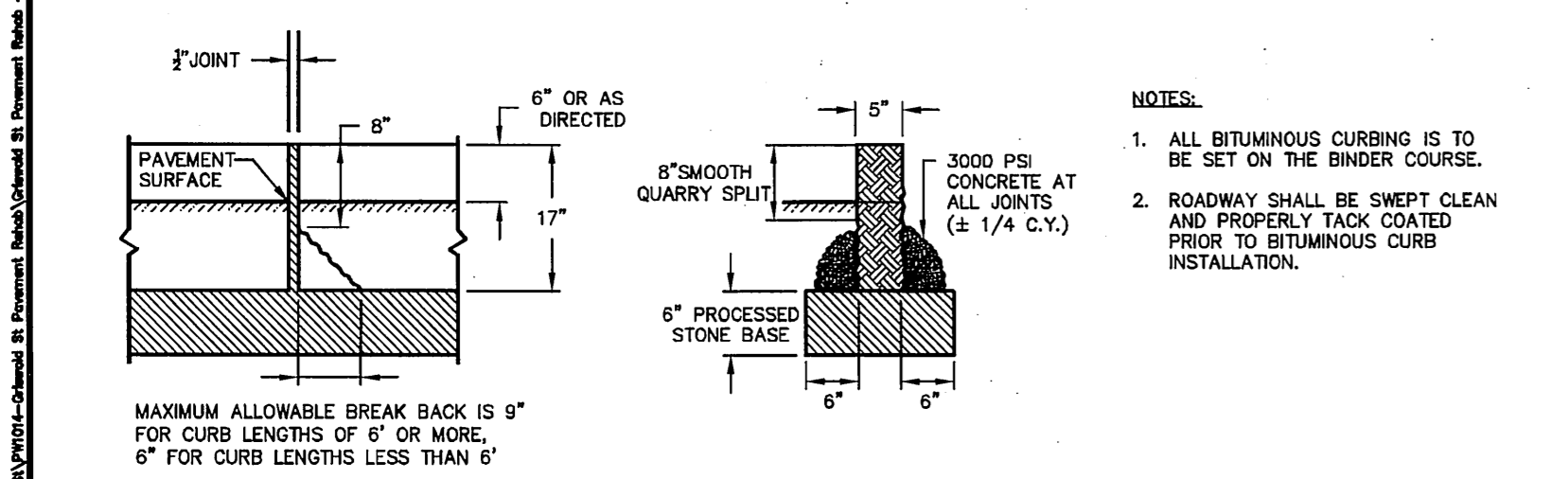
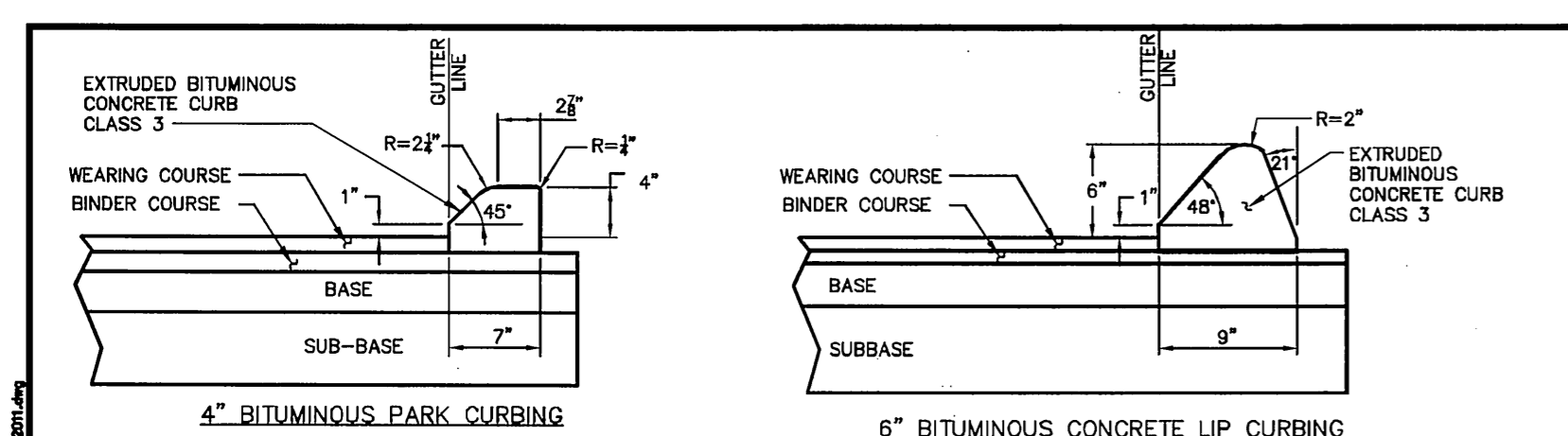
FULL DEPTH RECONSTRUCTION
TYPICAL CROSS SECTION
STA. 4+50 to STA. 6+25, STA. 15+90 to STA. 18+00,
STA. 20+00 to STA. 23+75
NOT TO SCALE



FINE MILLING W/ 3" OVERLAY
TYPICAL CROSS SECTION
STA. 2+75 to STA. 4+50, STA. 6+25 to STA. 15+90
& STA. 18+00 to STA. 20+00
NOT TO SCALE



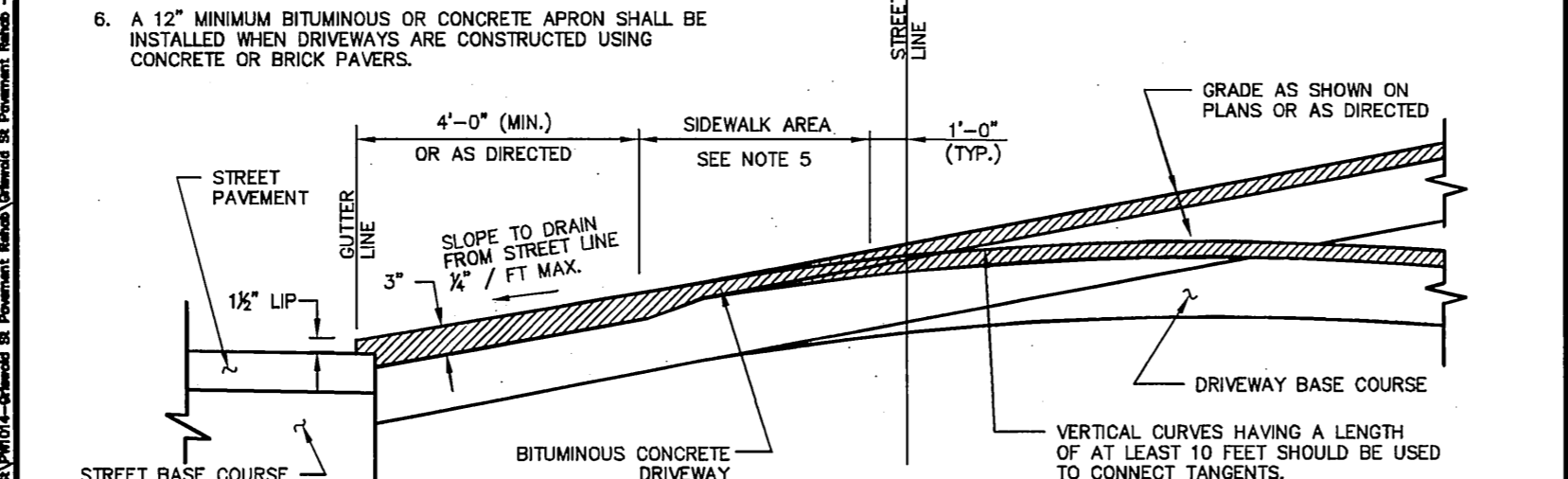
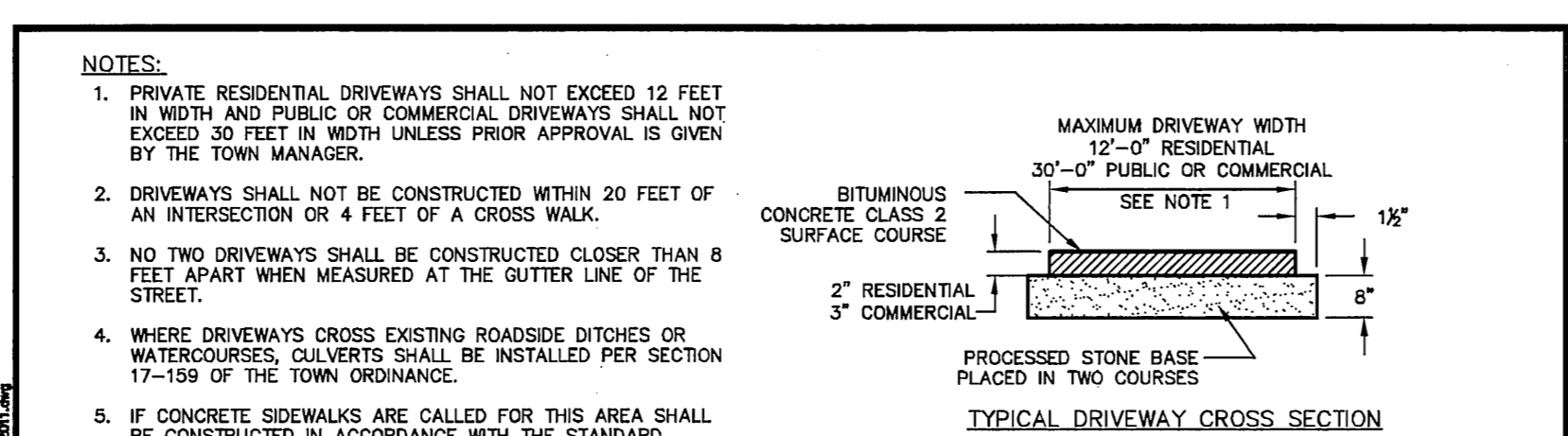
FINE MILLING W/ 2" OVERLAY
TYPICAL CROSS SECTION
STA. 23+75 to STA. 27+00, STA. 30+0 to STA. 68+50
NOT TO SCALE



SCALE: NONE
DRAWN BY: [Signature]
CHECKED BY: S.M.B.
APPROVED BY: D.A.P.
LAST REVISED: 4/29/2008

TOWN OF GLASTONBURY
DEPARTMENT OF PHYSICAL SERVICES
ENGINEERING DIVISION

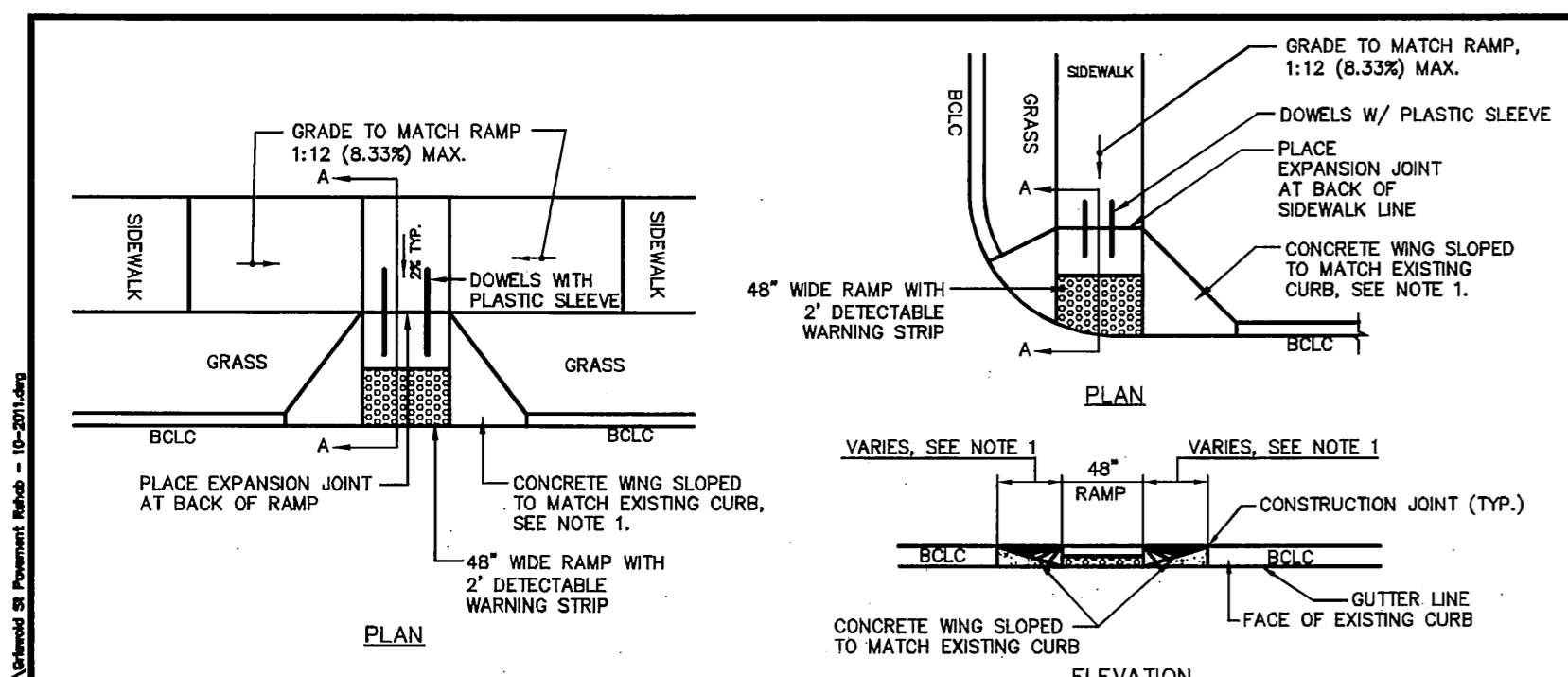
TYPICAL CURBING DETAILS
PLATE NO. 5



SCALE: NONE
DRAWN BY: [Signature]
CHECKED BY: S.M.B.
APPROVED BY: D.A.P.
LAST REVISED: 4/29/2008

TOWN OF GLASTONBURY
DEPARTMENT OF PHYSICAL SERVICES
ENGINEERING DIVISION

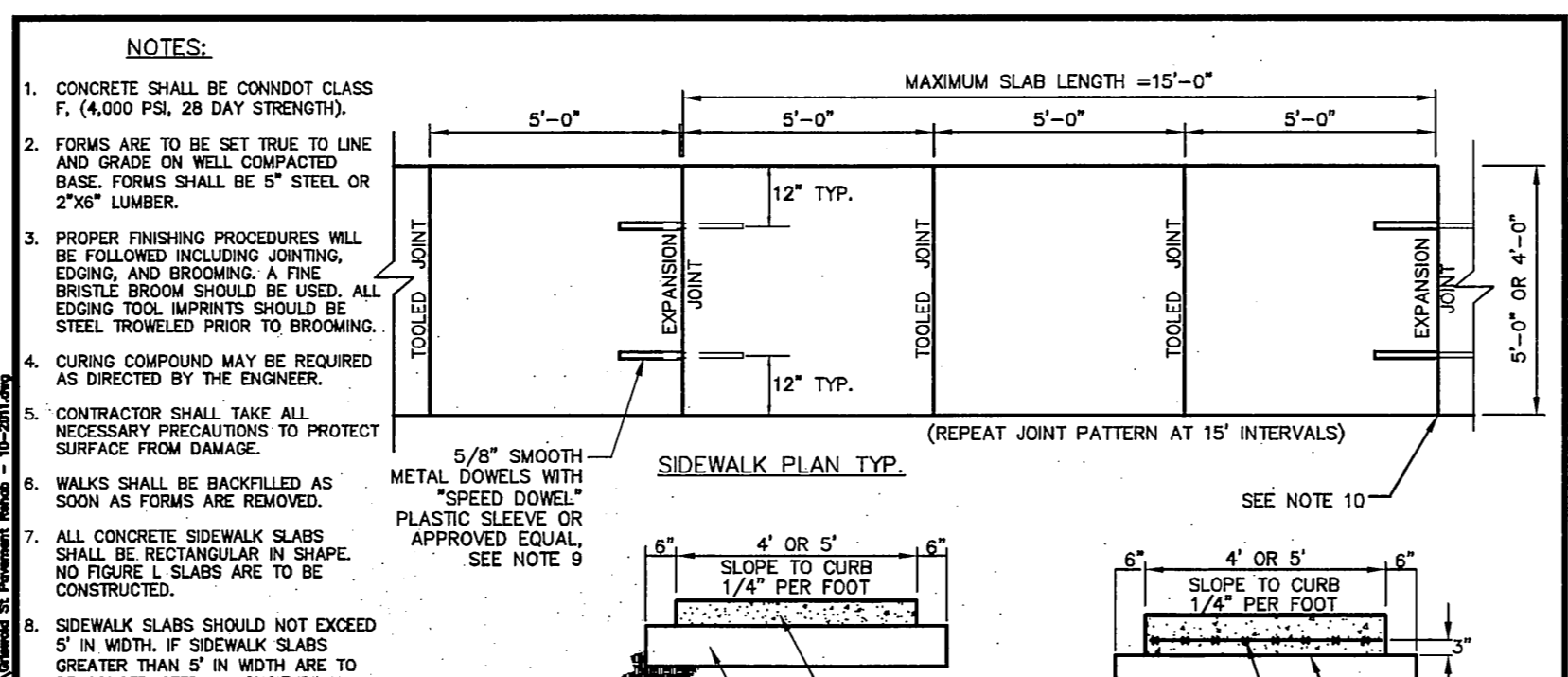
BITUMINOUS CONCRETE DRIVEWAY
PLATE NO. 6



SCALE: NONE
DRAWN BY: [Signature]
CHECKED BY: S.M.B.
APPROVED BY: D.A.P.
LAST REVISED: 2/4/2011

TOWN OF GLASTONBURY
DEPARTMENT OF PHYSICAL SERVICES
ENGINEERING DIVISION

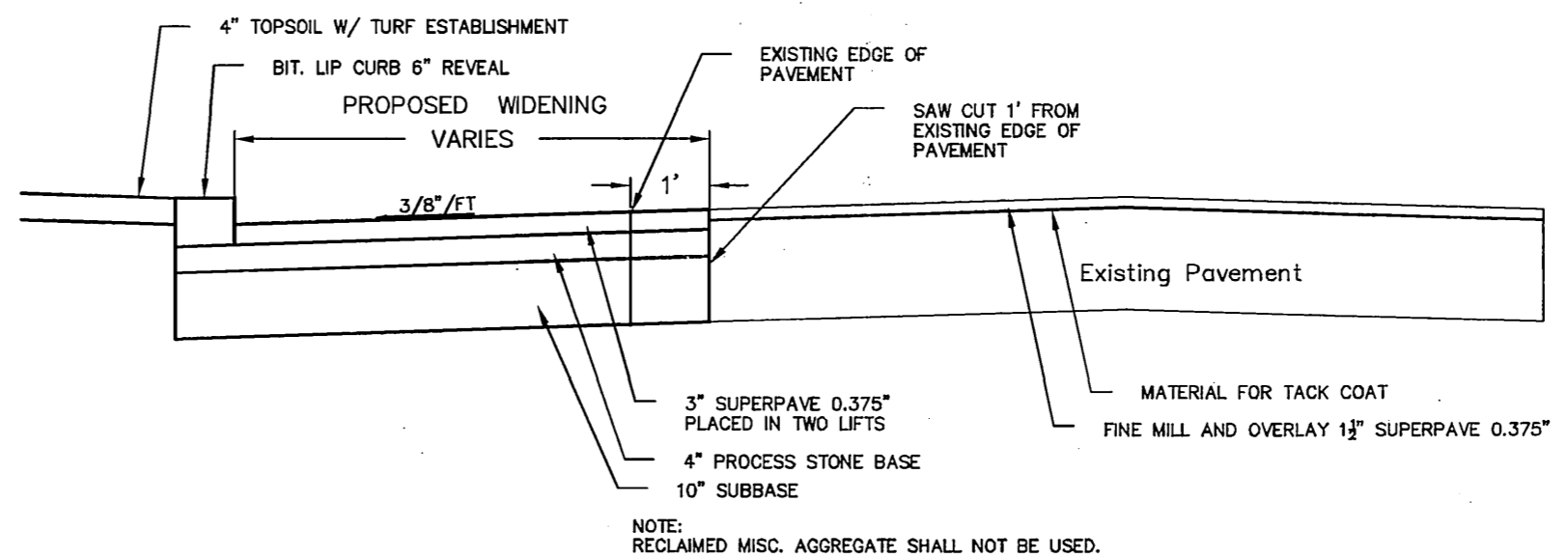
CONCRETE SIDEWALK RAMPS
PLATE NO. 7



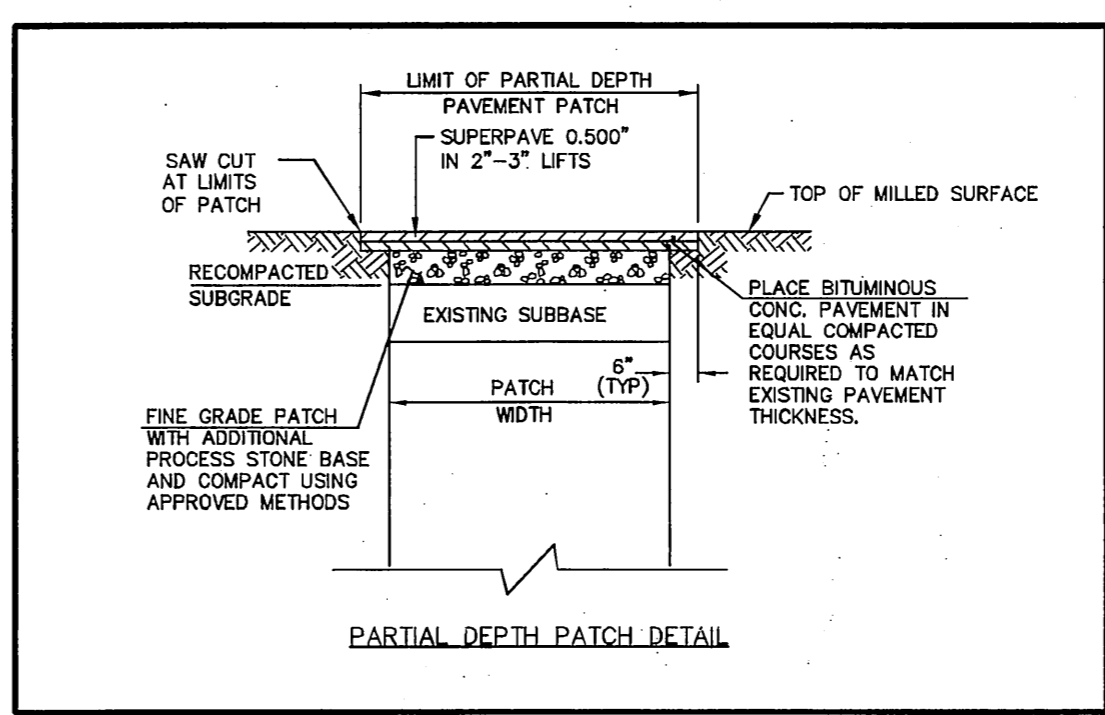
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DRAWN BY: [Signature]
CHECKED BY: S.M.B.
APPROVED BY: D.A.P.
LAST REVISED: 2/4/2011

TOWN OF GLASTONBURY
DEPARTMENT OF PHYSICAL SERVICES
ENGINEERING DIVISION

CONCRETE SIDEWALKS
PLATE NO. 8

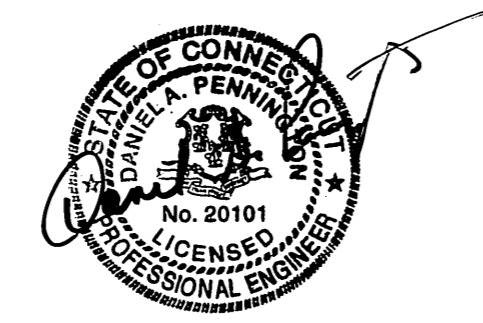


ROADWAY WIDENING AND
FINE MILLING W/ 1 1/2" OVERLAY
TYPICAL CROSS SECTION
CANDLEWOOD ROAD
STA. 0+16 to STA. 1+40
NOT TO SCALE



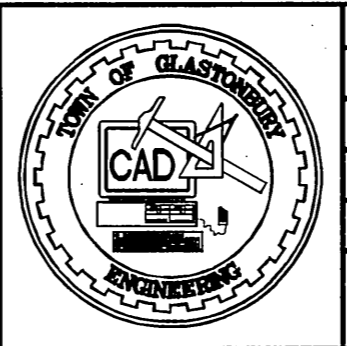
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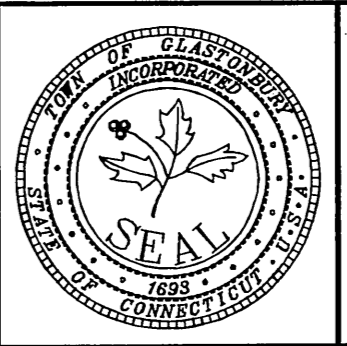
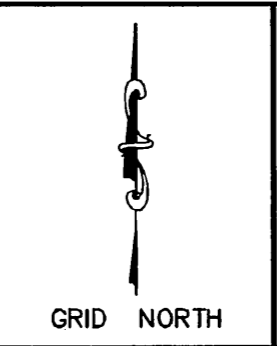


STP 53-184
PW-1014

DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011

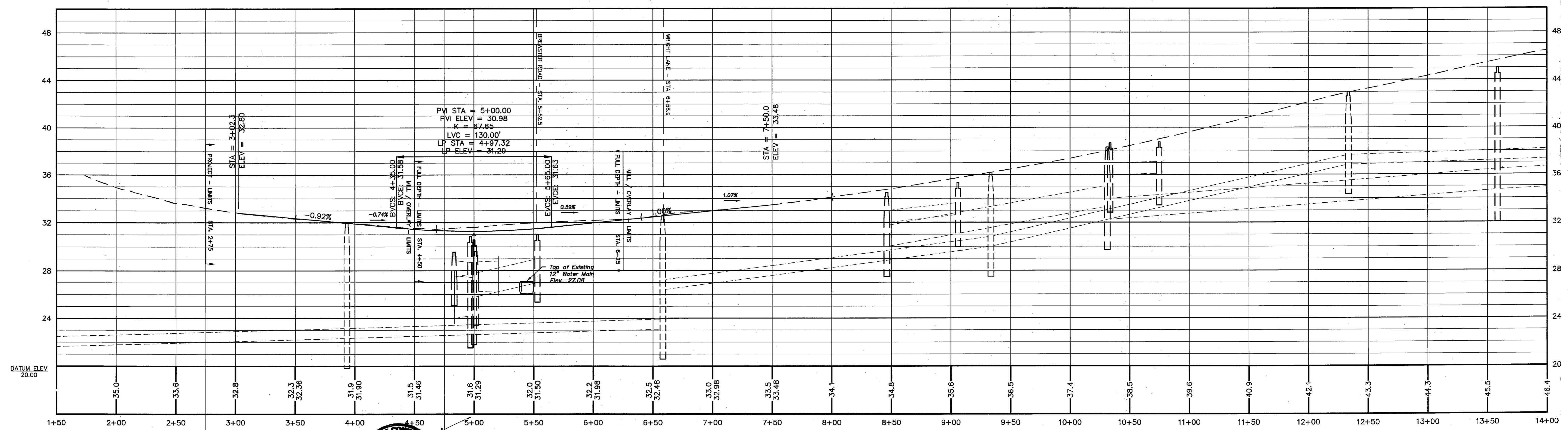
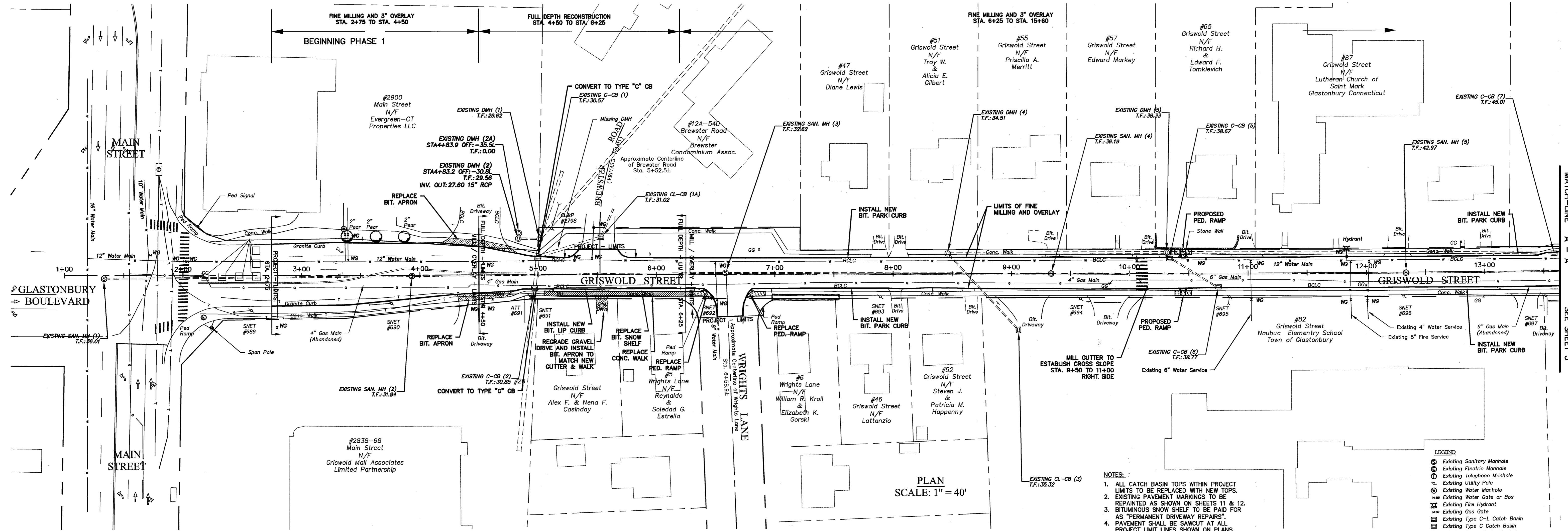


SCALE: AS SHOWN	DATE:
DRAWN BY: C.F.S.	1/11/2011
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	
DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.	



TYPICAL CROSS-SECTIONS & DETAILS
PAVEMENT REHABILITATION
FOR
GRISWOLD STREET
LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

FILE:R:\DWG\Streets\Griswold\STP\1014-Griswold St Pavement Rehab-10-2011.dwg USER: Charles Stuehlinger DATE: 11/08/2011



NOTE:
 LOCATION OF FEATURES AND CONTOUR DATA DEPICTED HEREON WERE ACQUIRED BY FIELD SURVEY ON DECEMBER 2010. THERE IS NO BOUNDARY/DETERMINATION OPINION. NO EASEMENTS PERTAINING TO THE SUBJECT PROJECT ARE DEPICTED HEREON. NOT ALL IMPROVEMENTS ARE DEPICTED HEREON. HORIZONTAL CONTROL IS BASED ON THE CONNECTICUT GEODETIC SURVEY STATE PLANE COORDINATES, NORTH AMERICAN DATUM OF 1983 - (NAD83). ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).

PROFILING ENGINEER

SEAL

GRAPHIC SCALE

(IN FEET)

1 inch = 40 ft

1" = 40' HORIZ SCALE

1" = 4' VERT SCALE

IF SCALING FROM THIS PLAN, VERIFY SCALE WITH THE GRAPHIC SCALE PROVIDED ABOVE.

STP 53-184
PW-1014

DRAWING ISSUE STATUS		
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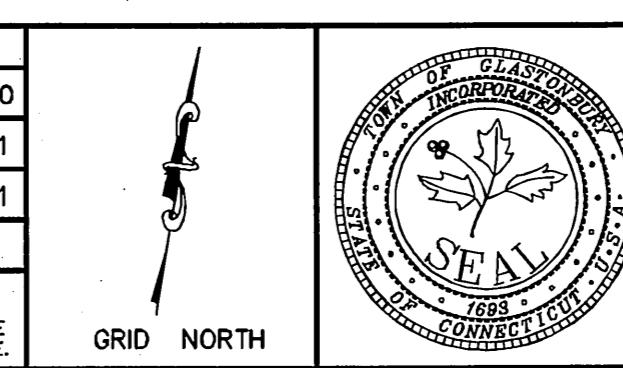
DRAWN BY: C.F.S.

CHECKED BY: S.M.B. 7/11/2011

APPROVED BY: D.A.P. 7/11/2011

ST. FILE:

DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.

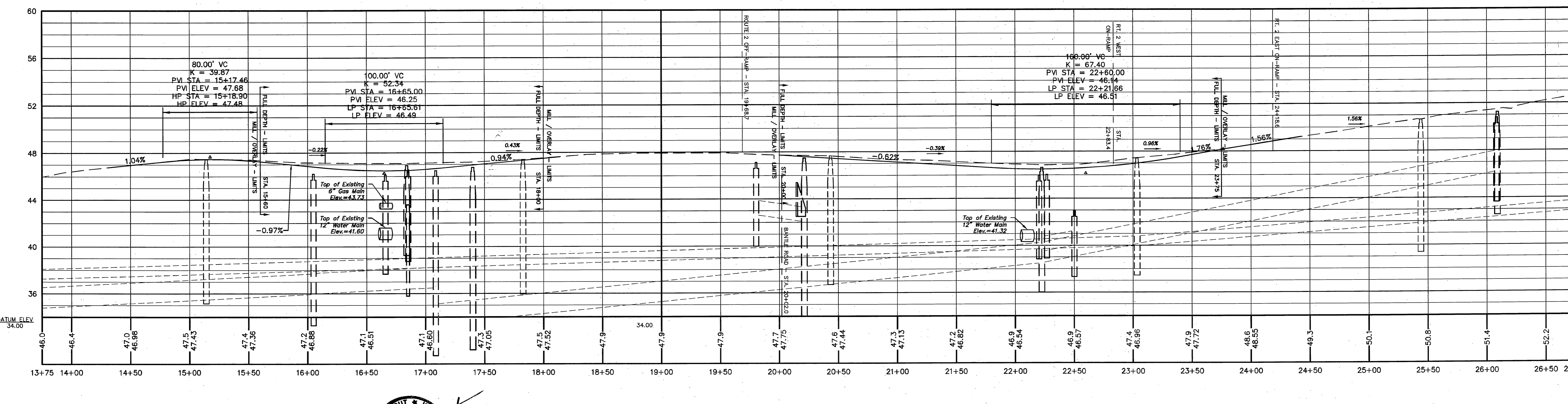
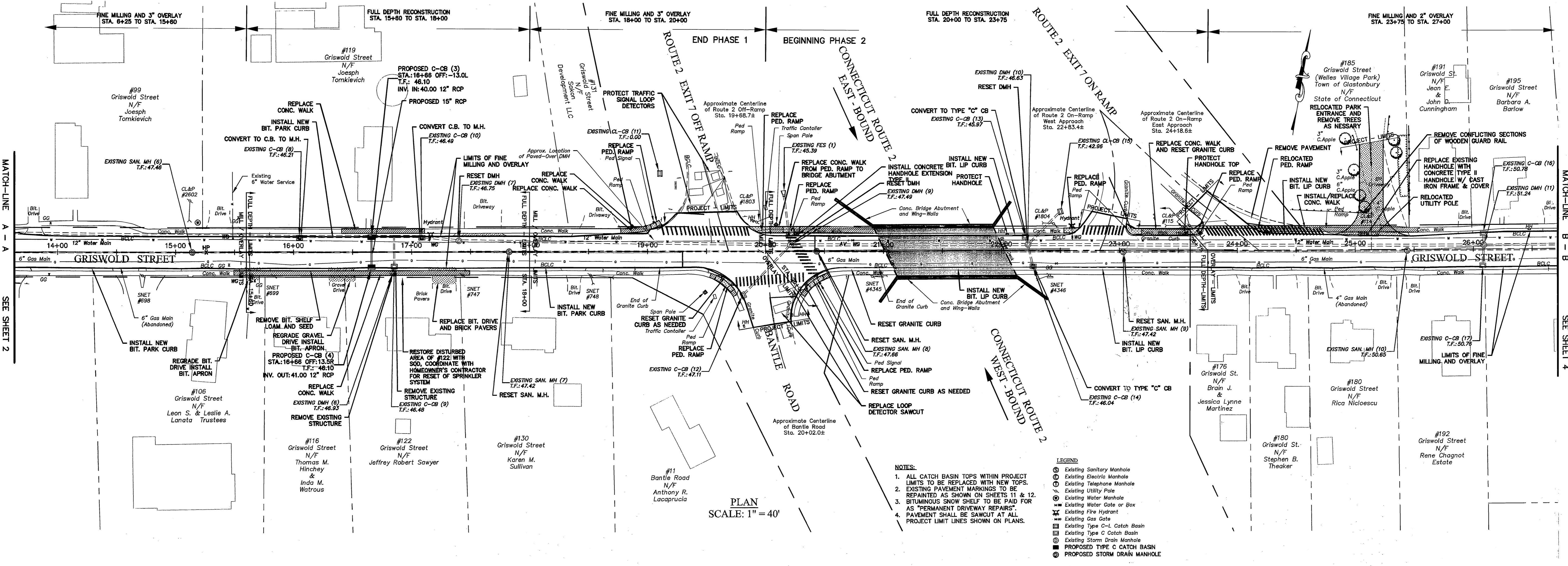


PLAN & PROFILE PAVEMENT REHABILITATION FOR GRISWOLD STREET

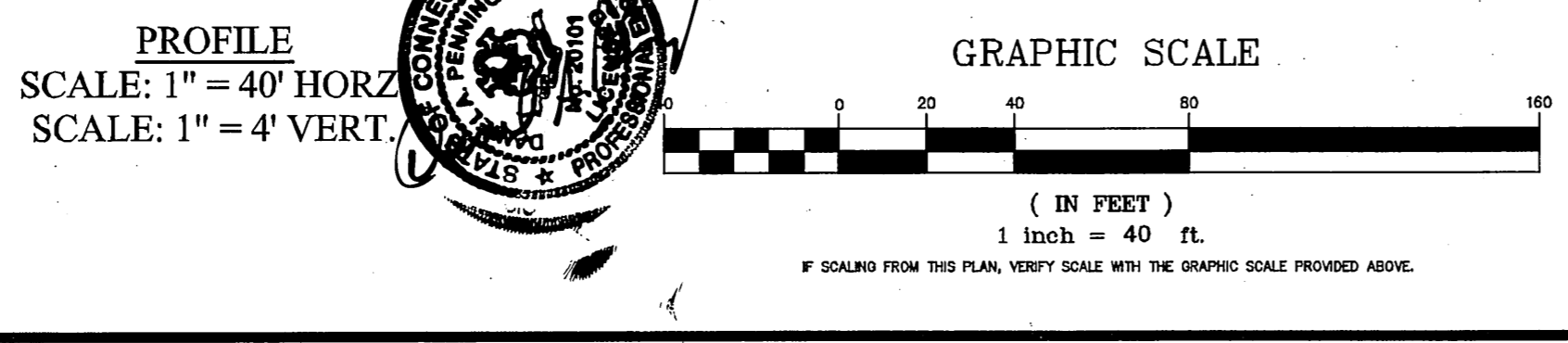
LOCATED BETWEEN MAIN STREET & CANDLEWOOD ROAD

GLASTONBURY, CONNECTICUT

FILE: H:\DWG\Streets\Griswold ST\PW1014-Griswold ST\Drawings\Rehab\Griswold ST\Drawings\Rehab - 10/2011.dwg (USER: Charles Stamblyger DATE: 1/23/2011)

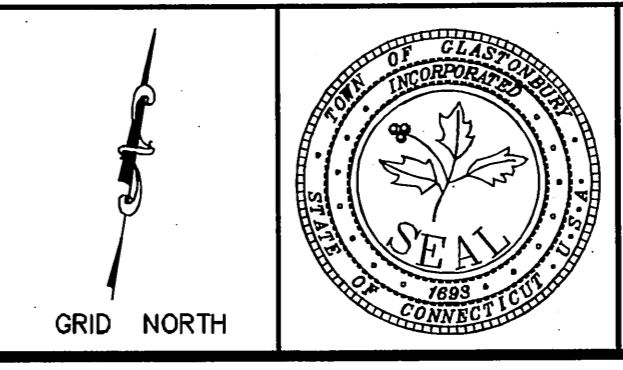


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 ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).



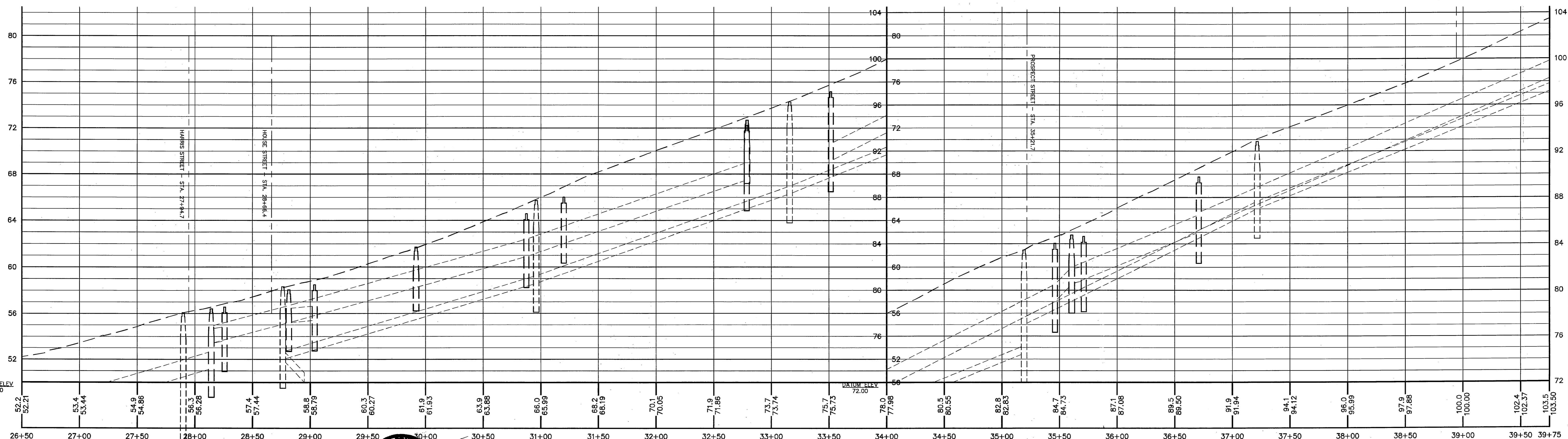
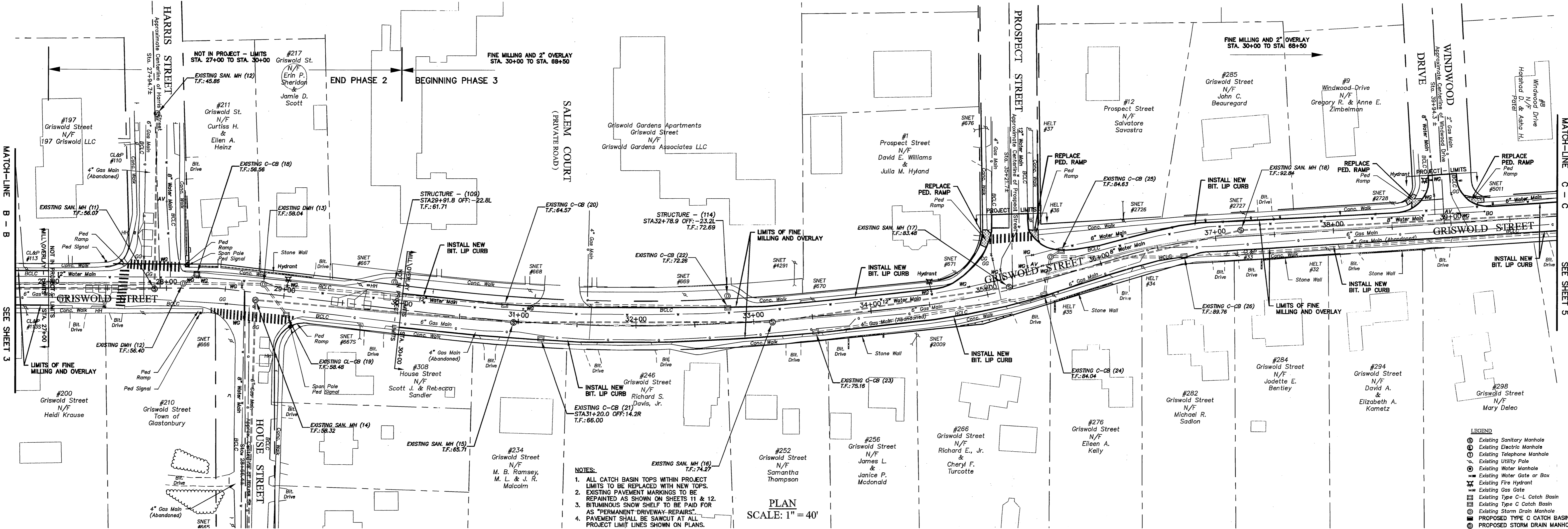
DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011

SCALE: AS SHOWN DATE: 11/11/2010
 DRAWN BY: C.F.S. 7/11/2010
 CHECKED BY: S.M.B. 7/11/2011
 APPROVED BY: D.A.P. 7/11/2011
 ST. FILE:
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PLAN & PROFILE
 PAVEMENT REHABILITATION
 FOR
 GRISWOLD STREET
 LOCATED BETWEEN
 MAIN STREET & CANDLEWOOD ROAD
 GLASTONBURY, CONNECTICUT

FILE: H:\PROJECTS\Griswold\STP53-184-Griswold-St-Pavement-Rehab-Griswold-St-Pavement-Rehab-10-30-11.dwg USER: Charis Stachliger DATE: 11/28/2011



NOTE:
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 ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).

PROFILE SCALE: 1" = 40' HORIZ SCALE: 1" = 4' VERT

GRAPHIC SCALE (IN FEET)

1 inch = 40 feet

0 20 40 80 160

SEAL OF THE TOWN OF GLASTONBURY

STP 53-184 PW-1014

DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011

SEAL OF THE TOWN OF GLASTONBURY

SCALE: AS SHOWN DATE: 11/11/2010

DRAWN BY: C.F.S.

CHECKED BY: S.M.B. 7/11/2011

APPROVED BY: D.A.P. 7/11/2011

ST. FILE:

DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.

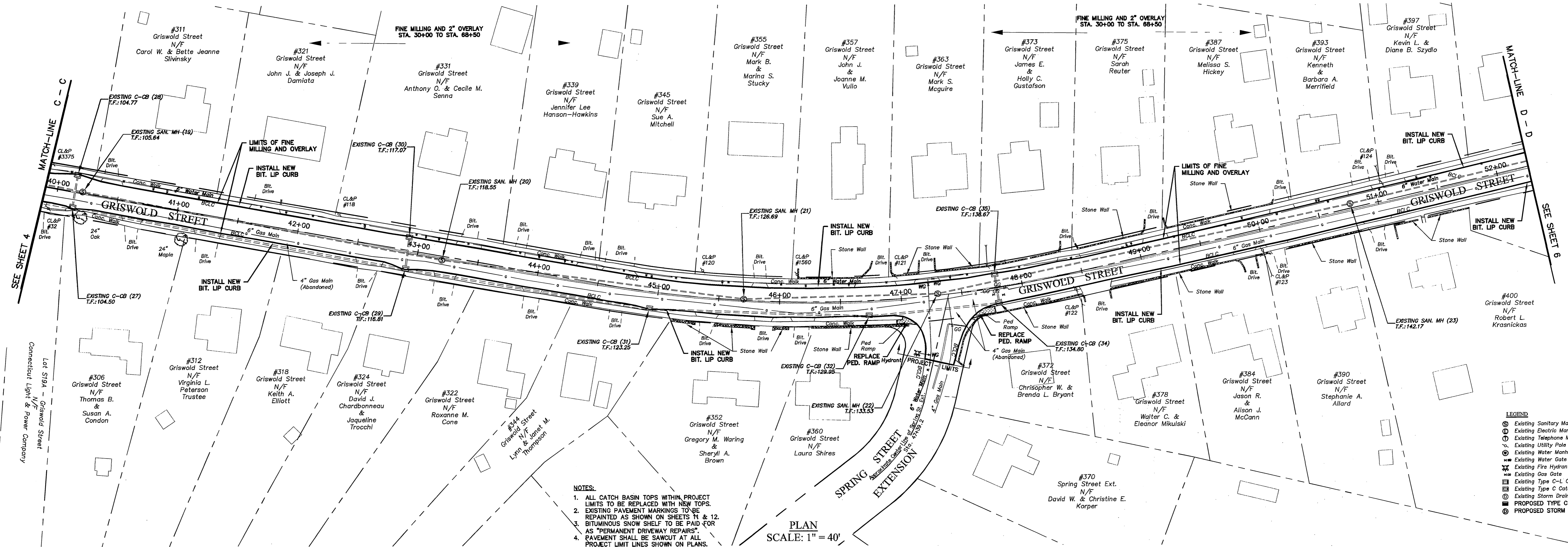
GRID NORTH

SEAL OF THE TOWN OF GLASTONBURY

PLAN & PROFILE PAVEMENT REHABILITATION FOR GRISWOLD STREET LOCATED BETWEEN MAIN STREET & CANDLEWOOD ROAD GLASTONBURY, CONNECTICUT

SHEET NO. 7 OF 15

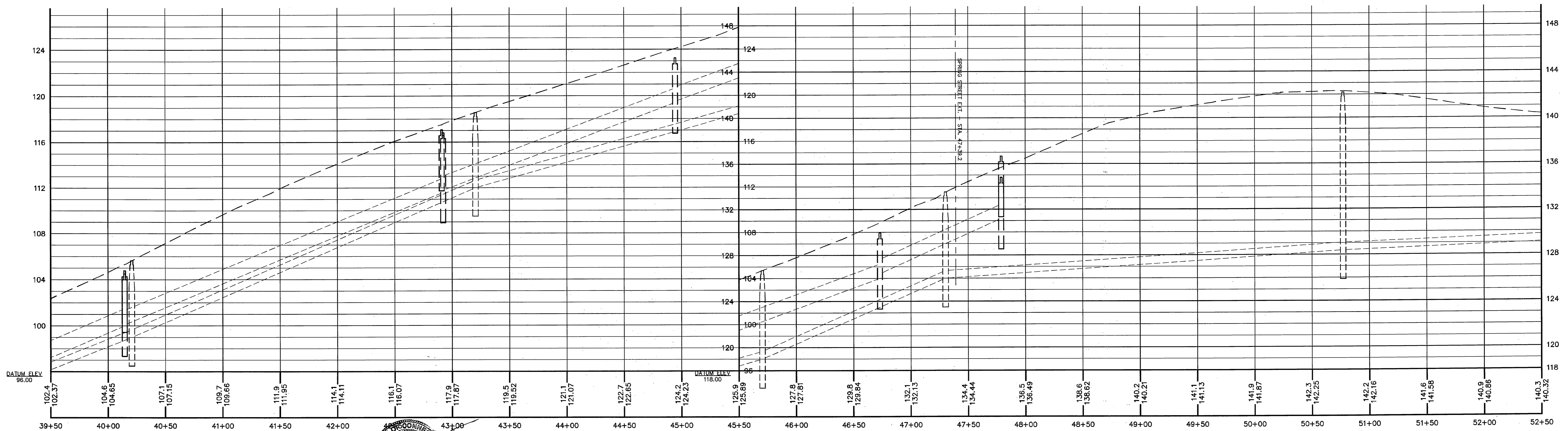
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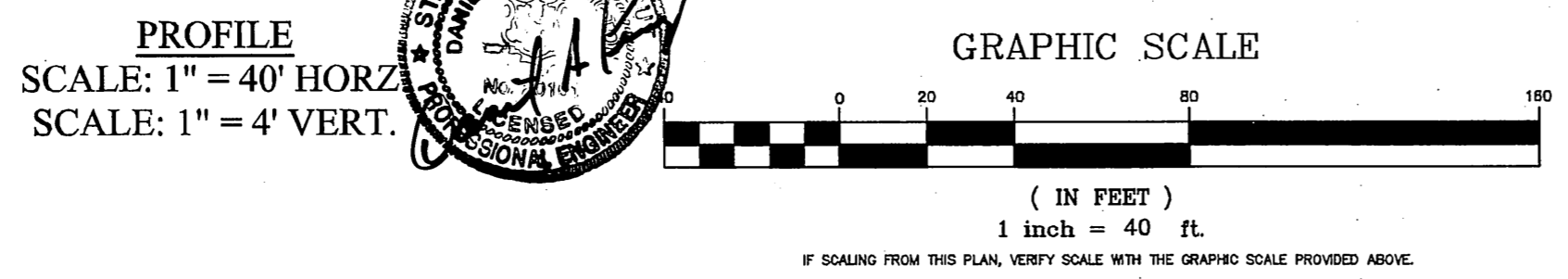
NOTES:
 1. ALL CATCH BASIN TOPS WITHIN PROJECT LIMITS TO BE REPLACED WITH NEW TOPS.
 2. EXISTING PAVEMENT MARKINGS TO BE REPAIRED AS SHOWN ON SHEETS 11 & 12.
 3. BITUMINOUS SNOW SHELF TO BE PAID FOR AS "PERMANENT DRIVEWAY REPAIRS".
 4. PAVEMENT SHALL BE SAWCUT AT ALL PROJECT LIMIT LINES SHOWN ON PLANS.

PLAN SCALE: 1" = 40'

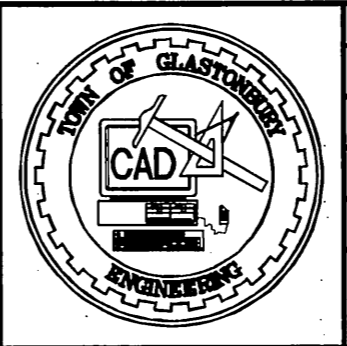
- LEGEND**
- ⊙ Existing Sanitary Manhole
 - ⊙ Existing Electric Manhole
 - ⊙ Existing Telephone Manhole
 - ⊙ Existing Utility Pole
 - ⊙ Existing Water Manhole
 - ⊙ Existing Water Gate or Box
 - ⊙ Existing Fire Hydrant
 - ⊙ Existing Gas Gate
 - ⊙ Existing Type C-L Catch Basin
 - ⊙ Existing Type C Catch Basin
 - ⊙ Existing Storm Drain Manhole
 - ⊙ PROPOSED TYPE C CATCH BASIN
 - ⊙ PROPOSED STORM DRAIN MANHOLE



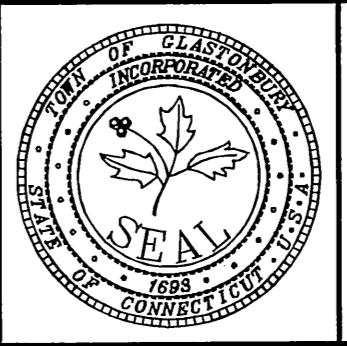
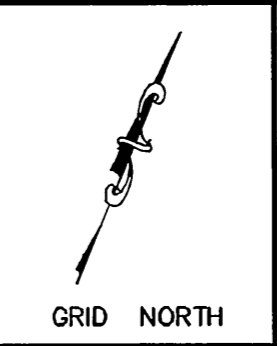
NOTE:
 LOCATION OF FEATURES AND CONTOUR DATA DEPICTED HEREON WERE ACQUIRED BY FIELD SURVEY ON DECEMBER 2010.
 THERE IS NO BOUNDARY/DETERMINATION OPINION.
 NO EASEMENTS PERTAINING TO THE SUBJECT PROJECT ARE DEPICTED HEREON.
 NOT ALL IMPROVEMENTS ARE DEPICTED HEREON.
 HORIZONTAL CONTROL IS BASED ON THE CONNECTICUT GEODETIC SURVEY STATE PLANE COORDINATES, NORTH AMERICAN DATUM OF 1983 - (NAD83).
 ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).



DRAWING ISSUE STATUS	
NO.	DESCRIPTION
1.	ISSUED FOR CONSTRUCTION
	DATE
	11/23/2011



SCALE: AS SHOWN	DATE:
DRAWN BY: C.F.S.	11/11/2010
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	

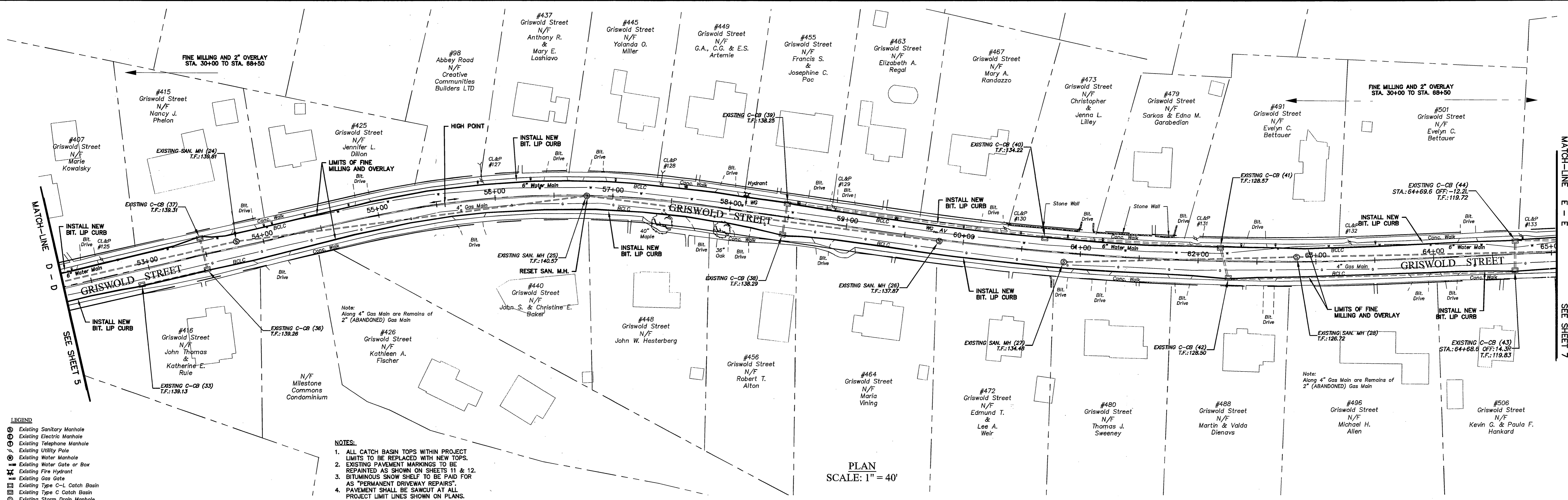


PLAN & PROFILE
PAVEMENT REHABILITATION
 FOR
GRISWOLD STREET
 LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

SHEET NO.
8
 OF 15

STP 53-184
 PW-1014

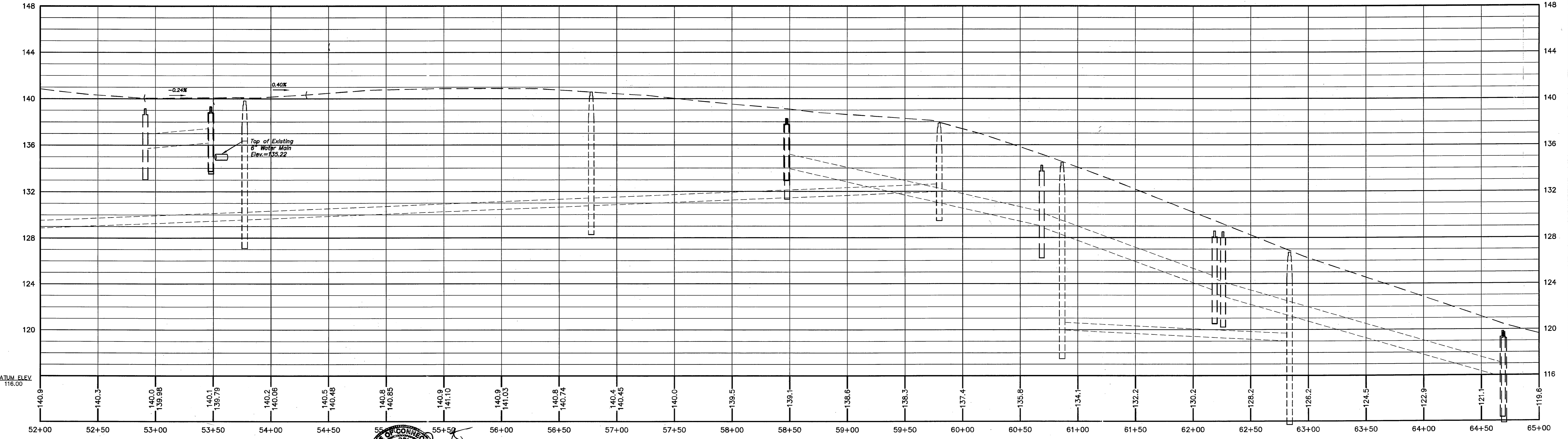
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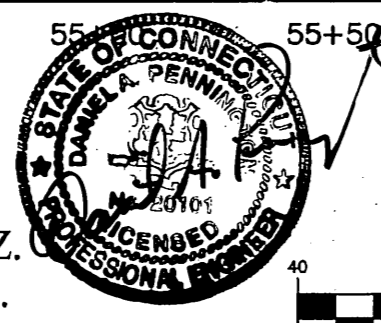
- LEGEND**
- ⊙ Existing Sanitary Manhole
 - ⊙ Existing Electric Manhole
 - ⊙ Existing Telephone Manhole
 - ⊙ Existing Utility Pole
 - ⊙ Existing Water Manhole
 - ⊙ Existing Water Gate or Box
 - ⊙ Existing Fire Hydrant
 - ⊙ Existing Gas Gate
 - ⊙ Existing Type C-1 Catch Basin
 - ⊙ Existing Type C Catch Basin
 - ⊙ Existing Storm Drain Manhole
 - ⊙ PROPOSED TYPE C CATCH BASIN
 - ⊙ PROPOSED STORM DRAIN MANHOLE

- NOTES:**
1. ALL CATCH BASIN TOPS WITHIN PROJECT LIMITS TO BE REPLACED WITH NEW TOPS.
 2. EXISTING PAVEMENT MARKINGS TO BE REPAINTED AS SHOWN ON SHEETS 11 & 12.
 3. BITUMINOUS SNOW SHELF TO BE PAID FOR AS "PERMANENT DRIVEWAY REPAIRS".
 4. PAVEMENT SHALL BE SAWCUT AT ALL PROJECT LIMIT LINES SHOWN ON PLANS.

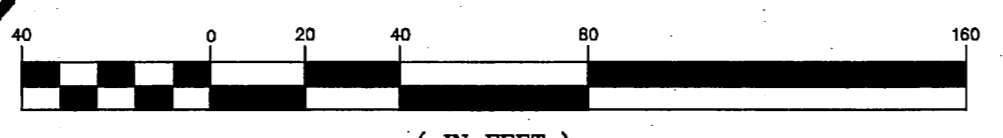
PLAN SCALE: 1" = 40'



PROFILE
SCALE: 1" = 40' HORZ.
SCALE: 1" = 4' VERT.



GRAPHIC SCALE



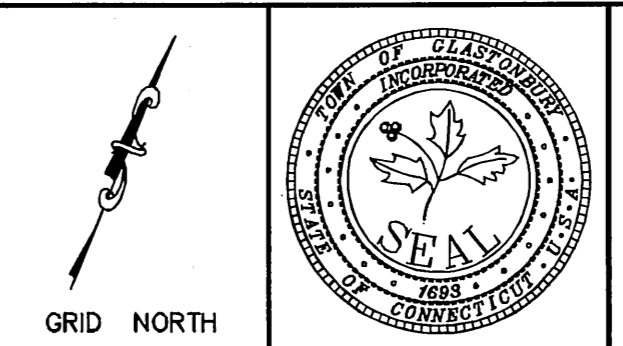
(IN FEET)
1 inch = 40 ft.
IF SCALING FROM THIS PLAN, VERIFY SCALE WITH THE GRAPHIC SCALE PROVIDED ABOVE.

NOTE:
LOCATION OF FEATURES AND CONTOUR DATA DEPICTED HEREON WERE ACQUIRED BY FIELD SURVEY ON DECEMBER 2010.
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ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).

STP 53-184
PW-1014

DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011

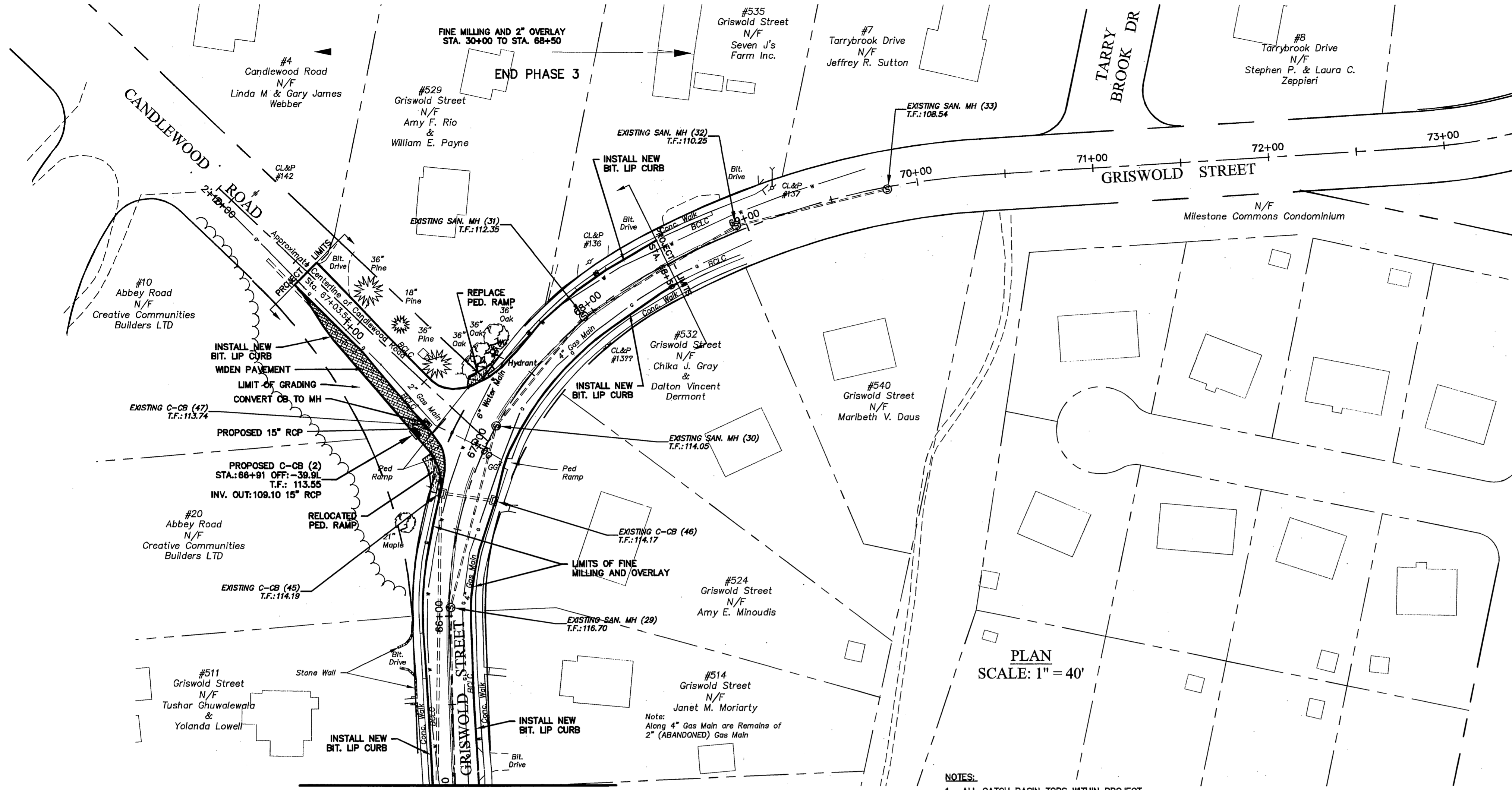
SCALE: AS SHOWN DATE: 11/11/2010
DRAWN BY: C.F.S.
CHECKED BY: S.M.B. 7/11/2011
APPROVED BY: D.A.P. 7/11/2011
ST. FILE:
DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.



PLAN & PROFILE
PAVEMENT REHABILITATION
FOR
GRISWOLD STREET
LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

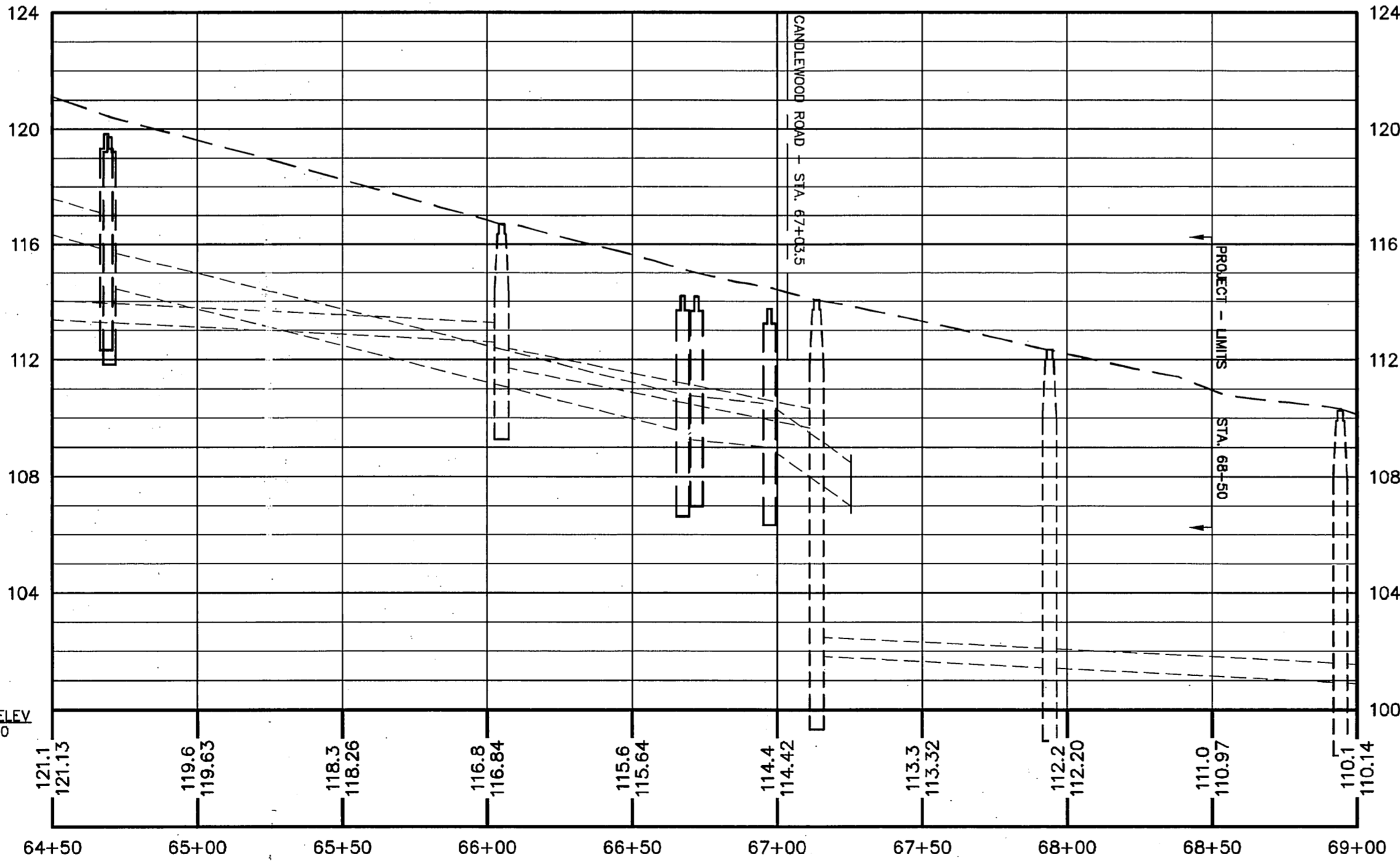
SHEET NO.
9
OF 15

FILE: H:\DWG\Shawn\Griswold\STP1014-Griswold St Pavement Rehab\Griswold St Pavement Rehab - 10-20-11.dwg USDR: Charles Stamboliger DATE: 11/28/2011



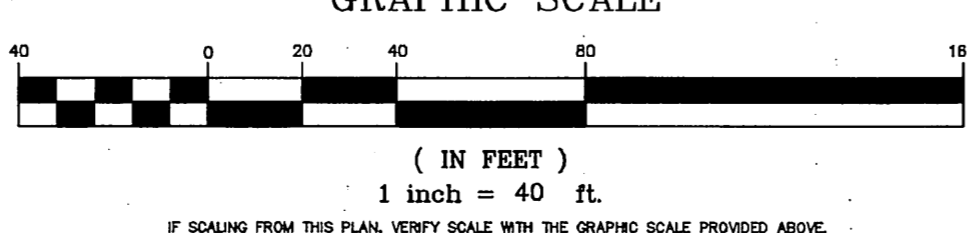
- LEGEND**
- ⊕ Existing Sanitary Manhole
 - ⊕ Existing Electric Manhole
 - ⊕ Existing Telephone Manhole
 - ⊕ Existing Utility Pole
 - ⊕ Existing Water Manhole
 - ⊕ Existing Water Gate or Box
 - ⊕ Existing Fire Hydrant
 - ⊕ Existing Gas Gate
 - ⊕ Existing Type C-1 Catch Basin
 - ⊕ Existing Type C Catch Basin
 - ⊕ Existing Storm Drain Manhole
 - ⊕ PROPOSED TYPE C CATCH BASIN
 - ⊕ PROPOSED STORM DRAIN MANHOLE

MATCH-LINE E - E SEE SHEET 6



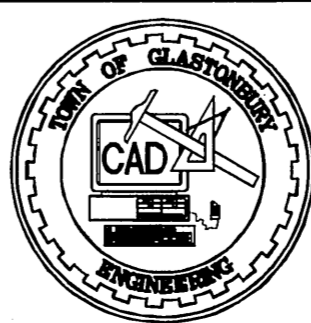
NOTE:

LOCATION OF FEATURES AND CONTOUR DATA DEPICTED HEREON WERE ACQUIRED BY FIELD SURVEY ON DECEMBER 2010. THERE IS NO BOUNDARY/DETERMINATION OPINION. NO EASEMENTS PERTAINING TO THE SUBJECT PROJECT ARE DEPICTED HEREON. NOT ALL IMPROVEMENTS ARE DEPICTED HEREON. HORIZONTAL CONTROL IS BASED ON THE CONNECTICUT GEODETIC SURVEY STATE PLANE COORDINATES, NORTH AMERICAN DATUM OF 1983 - (NAD83). ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).



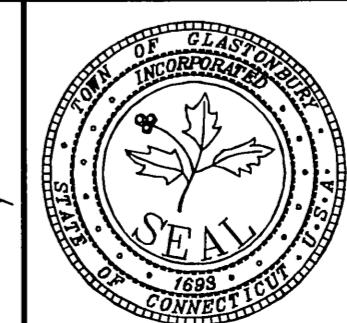
STP 53-184
PW-1014

DRAWING ISSUE STATUS	
NO.	DESCRIPTION
1.	ISSUED FOR CONSTRUCTION
	DATE: 11/23/2011



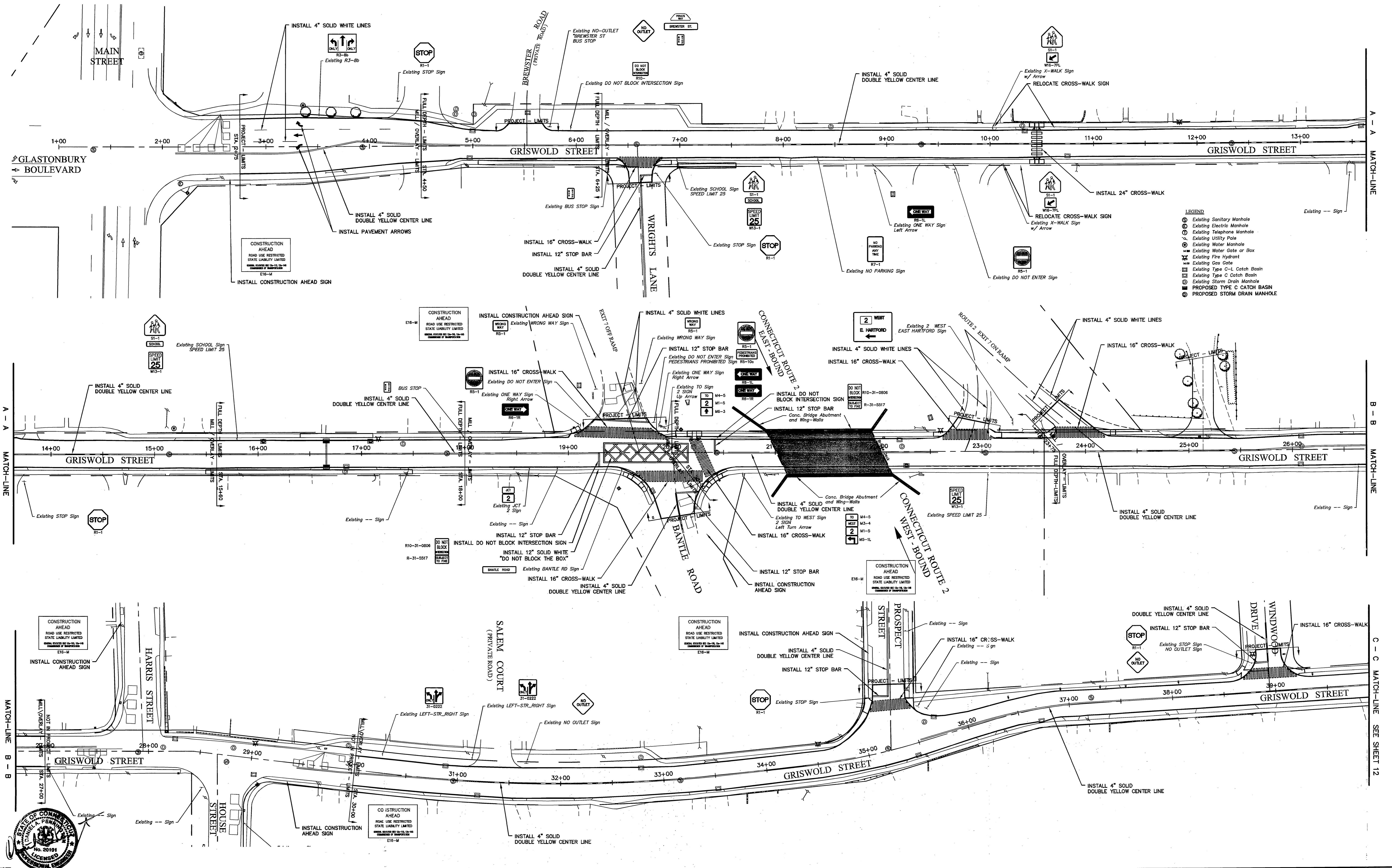
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DRAWN BY: C.F.S.	11/11/2010
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	

GRID NORTH



PLAN & PROFILE PAVEMENT REHABILITATION FOR GRISWOLD STREET
LOCATED BETWEEN MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

SHEET NO. **10** OF 15

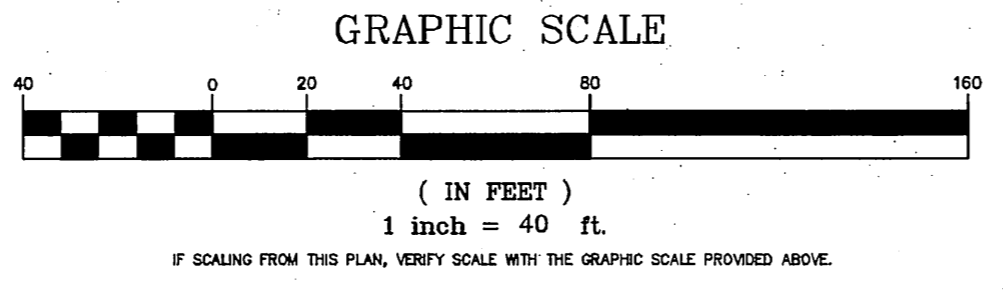


LEGEND

- ⊙ Existing Sanitary Manhole
- ⊙ Existing Electric Manhole
- ⊙ Existing Telephone Manhole
- ⊙ Existing Utility Pole
- ⊙ Existing Water Manhole
- ⊙ Existing Water Gate or Box
- ⊙ Existing Fire Hydrant
- ⊙ Existing Gas Gate
- ⊙ Existing Type C-L Catch Basin
- ⊙ Existing Type C Catch Basin
- ⊙ Existing Storm Drain Manhole
- ⊙ PROPOSED TYPE C CATCH BASIN
- ⊙ PROPOSED STORM DRAIN MANHOLE

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 ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAVD88).

NOTES:
 1. EXISTING PAVEMENT MARKINGS TO BE REPAINTED AS SHOWN ON THIS PLAN.
 2. SEE TRAFFIC MARKING PLAN SHEETS 11 & 12 FOR DETAILS.



STP 53-184
 PW-1014

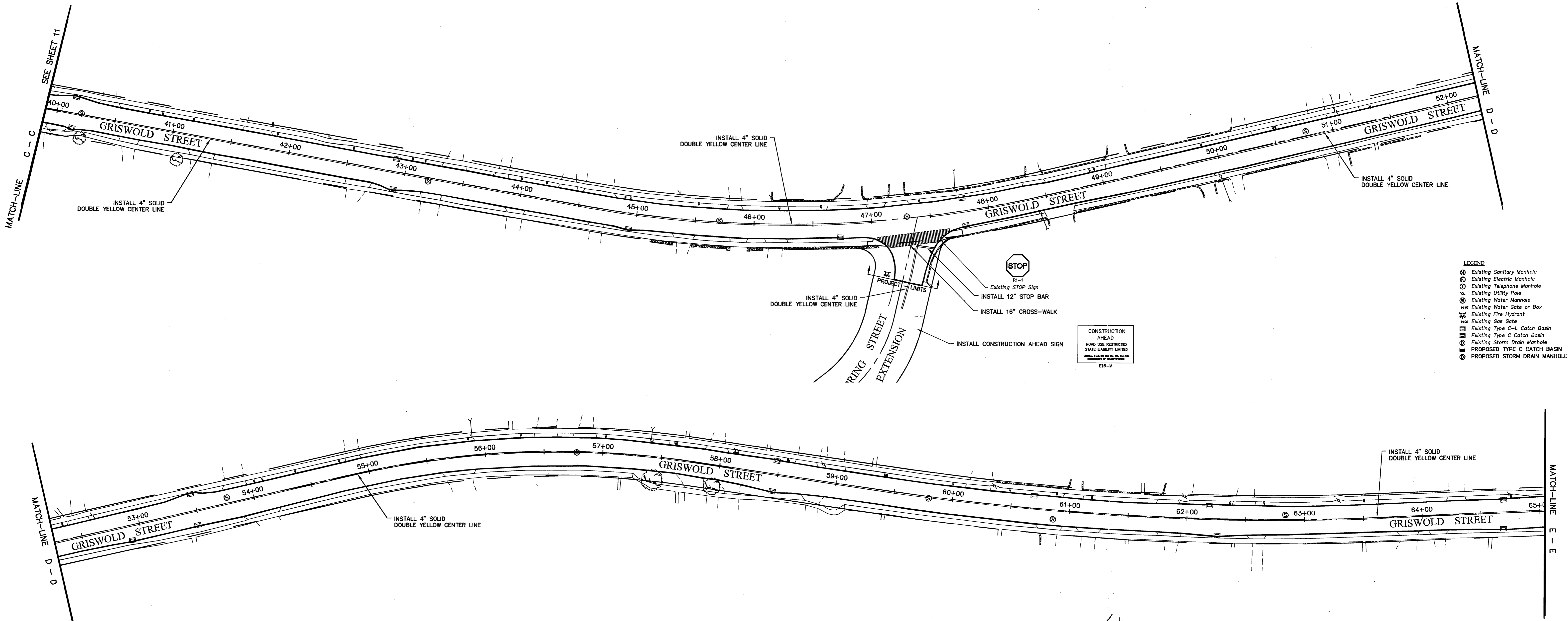
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NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011

SCALE: AS SHOWN DATE: 8/1/2011
 DRAWN BY: C.F.S. 8/1/2011
 CHECKED BY: S.M.B. 8/4/2011
 APPROVED BY: D.A.P. 8/4/2011
 ST. FILE:
 DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.

GRID NORTH

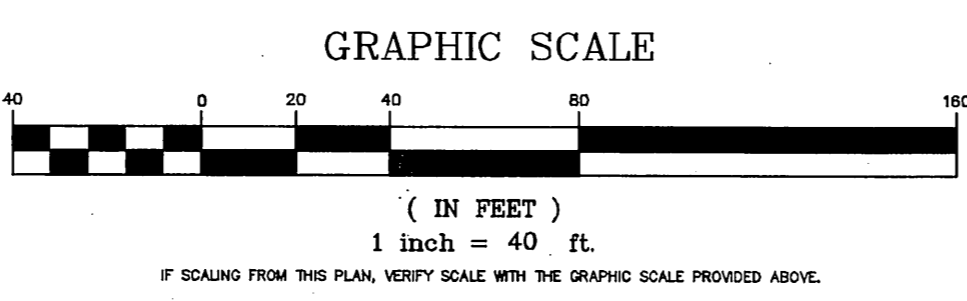
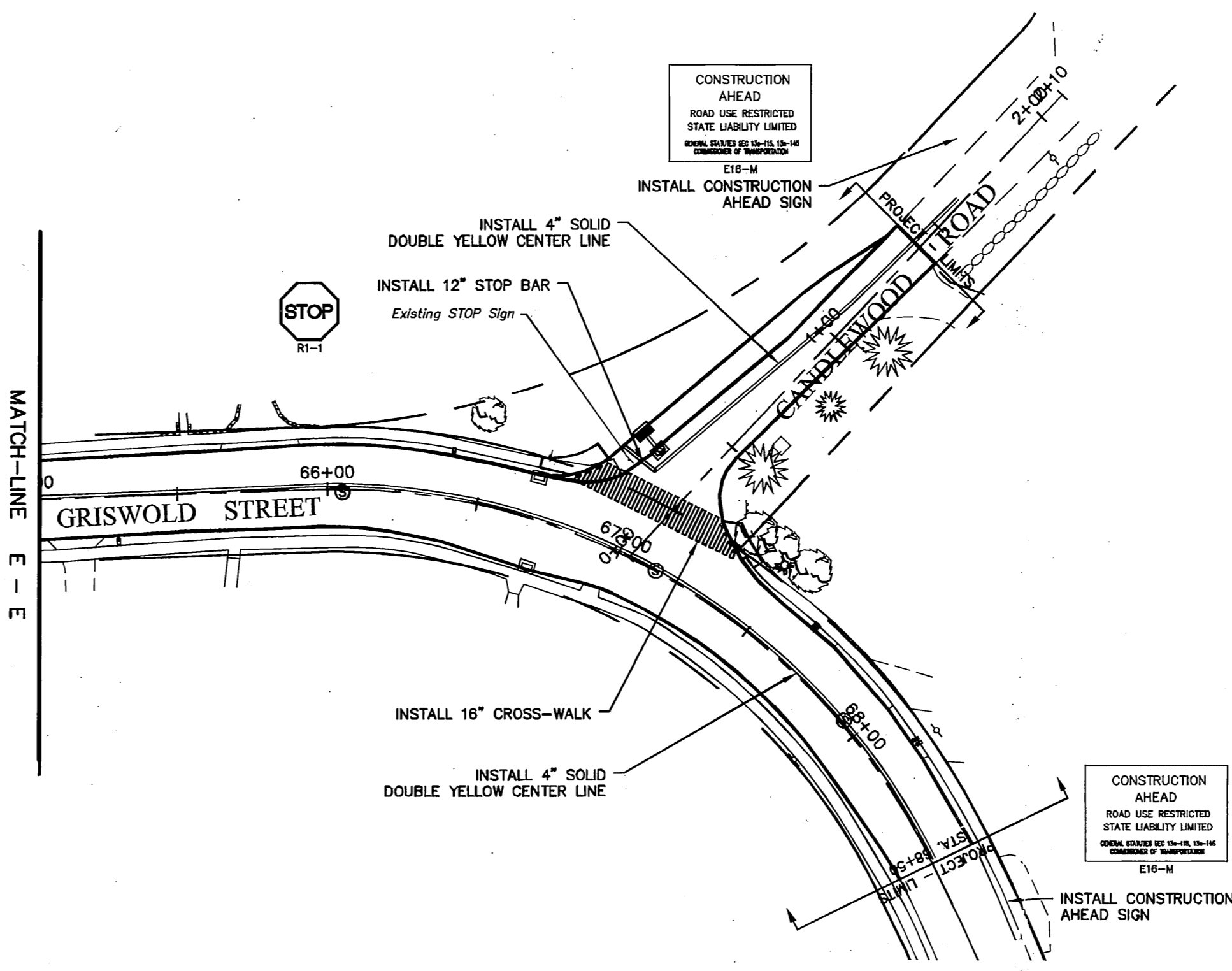
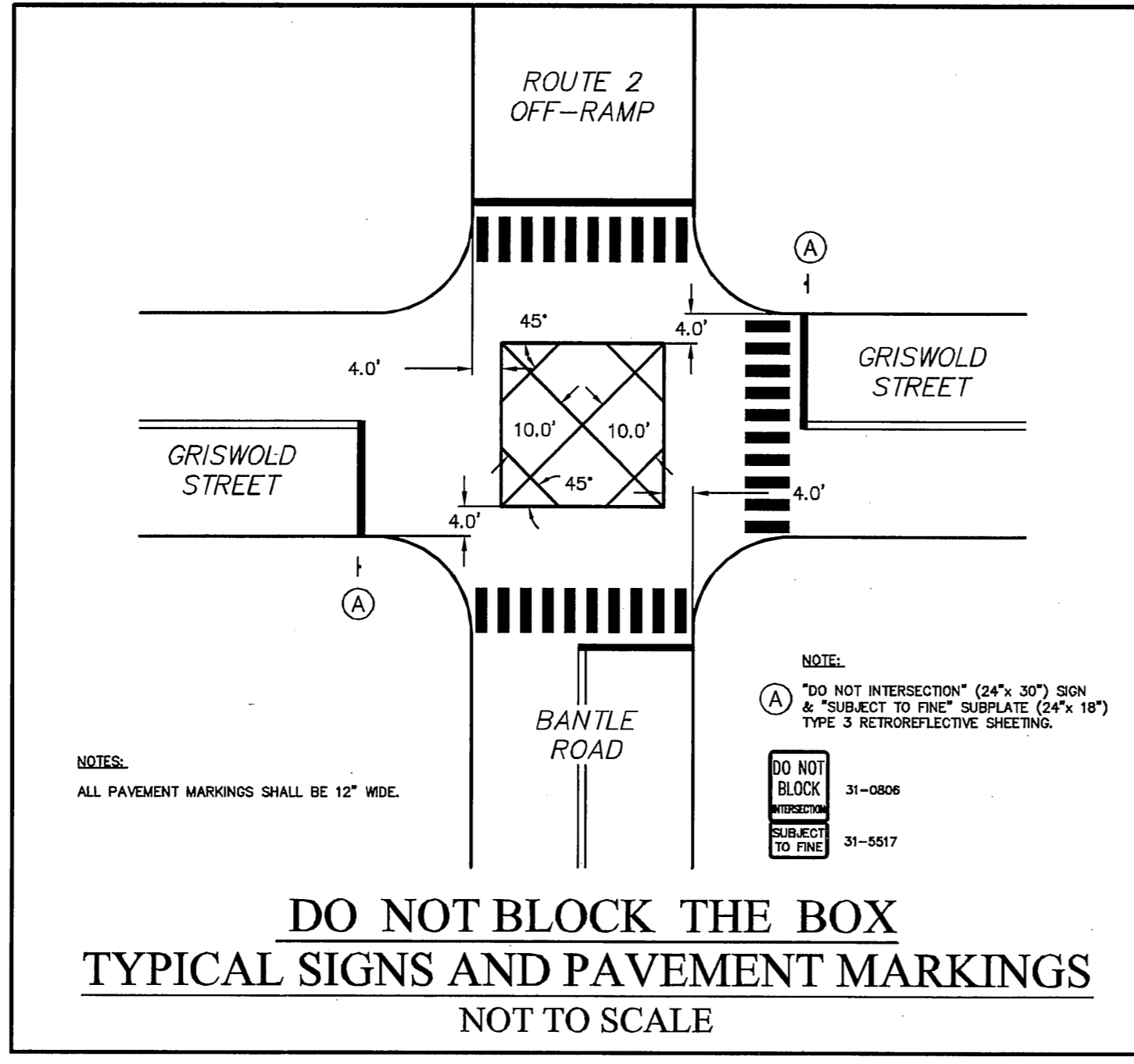
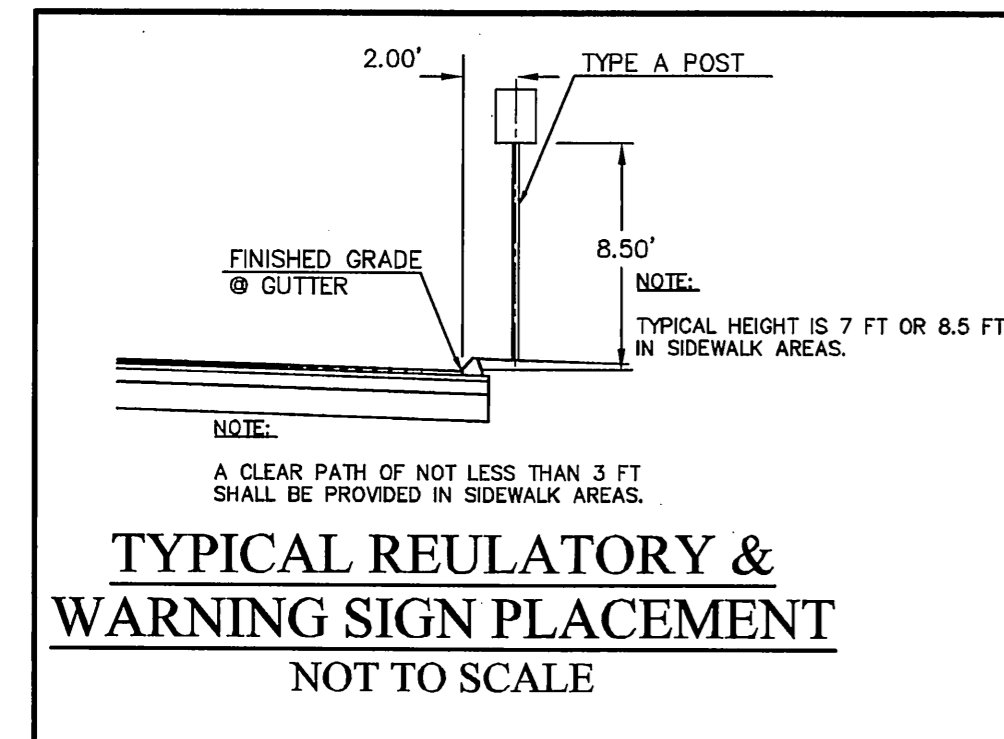
PAVEMENT MARKING PLAN
 PAVEMENT REHABILITATION
 FOR
 GRISWOLD STREET
 LOCATED BETWEEN
 MAIN STREET & CANDLEWOOD ROAD
 GLASTONBURY, CONNECTICUT

FILE: E:\DTPG\Streets\Griswold\STP1014-Griswold St Pavement Rehab\Griswold St Pavement Rehab - 10-2011.dwg USER: Charles Stubbler DATE: 11/28/2011

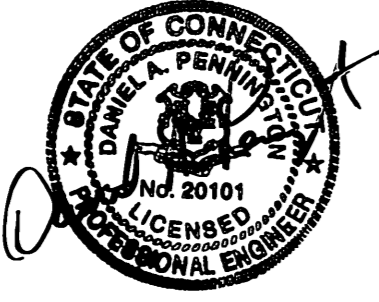


- LEGEND**
- ⊙ Existing Sanitary Manhole
 - ⊕ Existing Electric Manhole
 - ⊕ Existing Telephone Manhole
 - ⊕ Existing Utility Pole
 - ⊕ Existing Water Manhole
 - ⊕ Existing Water Gate or Box
 - ⊕ Existing Fire Hydrant
 - ⊕ Existing Gas Gate
 - ⊕ Existing Type C-L Catch Basin
 - ⊕ Existing Type C Catch Basin
 - ⊕ Existing Storm Drain Manhole
 - ⊕ PROPOSED TYPE C CATCH BASIN
 - ⊕ PROPOSED STORM DRAIN MANHOLE

CONSTRUCTION AHEAD
ROAD USE RESTRICTED
STATE LIABILITY LIMITED
E16-M



NOTE:
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ELEVATIONS AND VERTICAL CONTROL IS BASED ON THE NORTH AMERICAN VERTICAL DATUM 1988 - (NAV88).



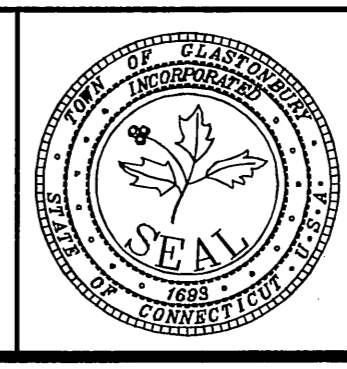
- NOTES:**
1. EXISTING PAVEMENT MARKINGS TO BE REPAINTED AS SHOWN ON THIS PLAN.
 2. SEE TRAFFIC MARKING PLAN SHEETS 11 & 12 FOR DETAILS.

STP 53-184
PW-1014

DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011
NO.	DESCRIPTION	DATE



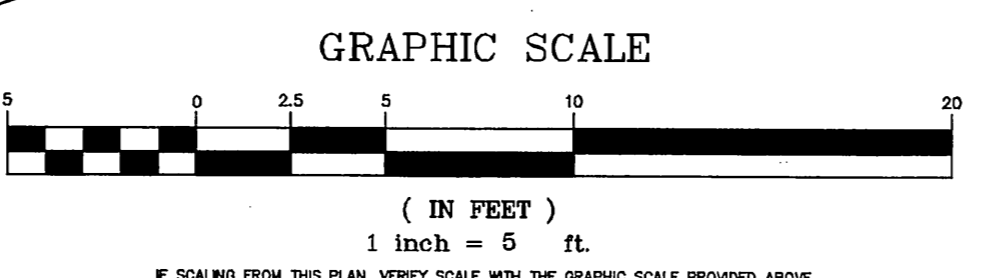
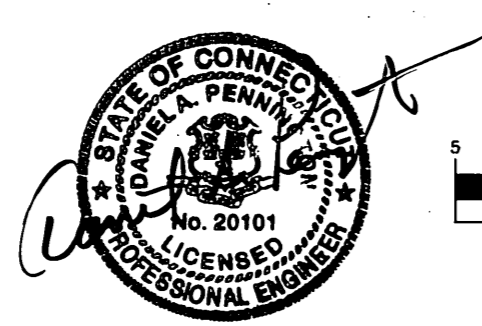
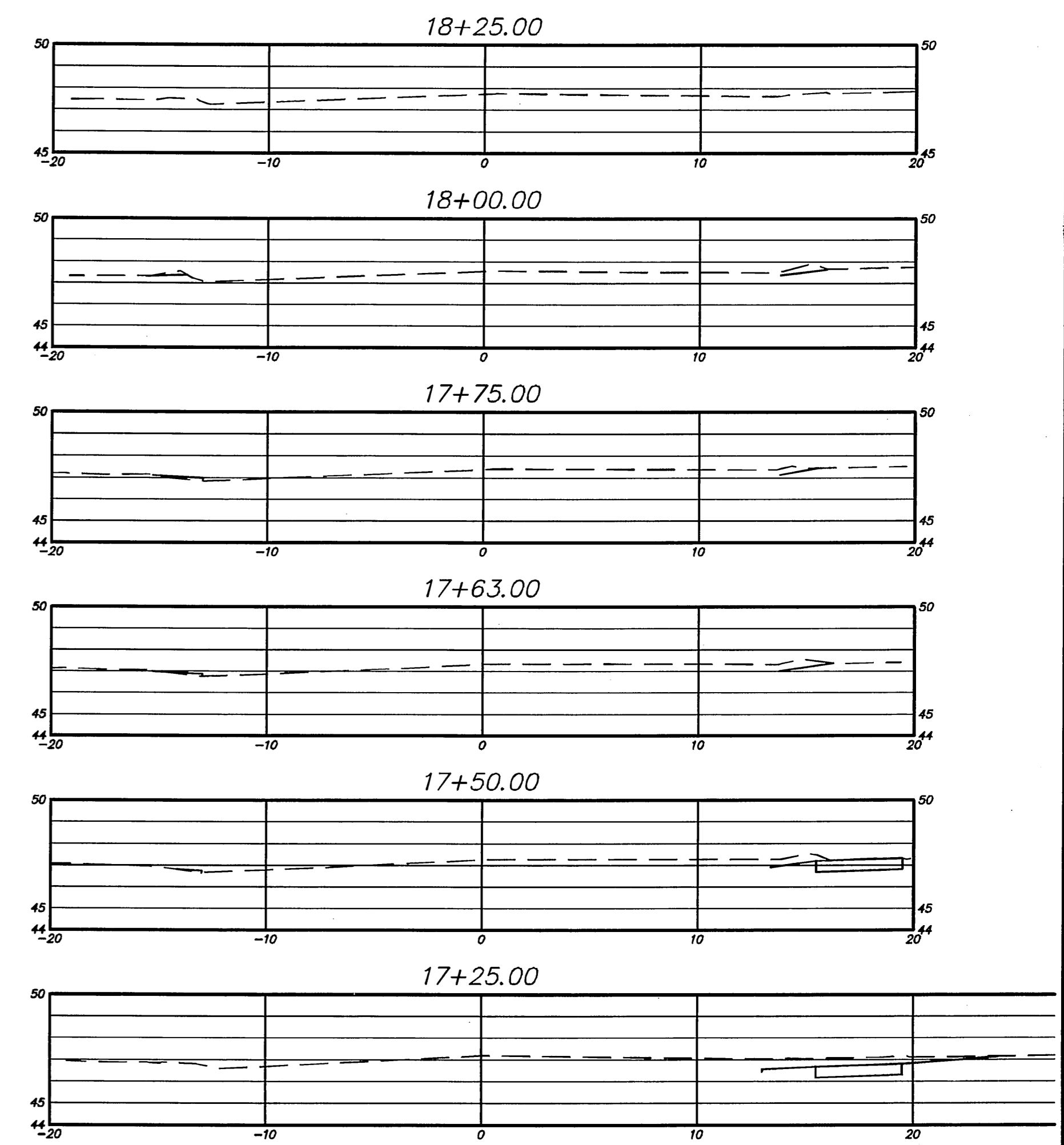
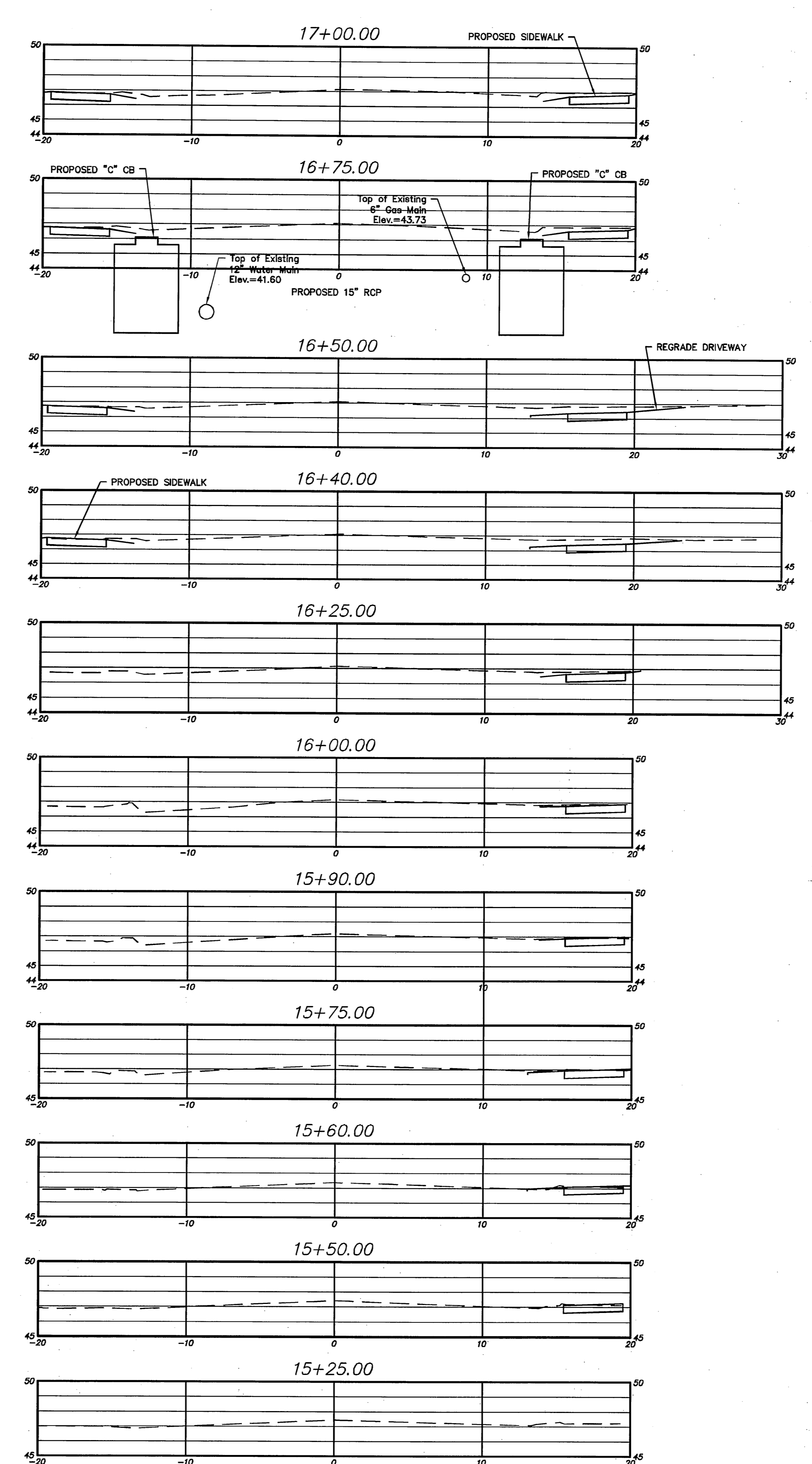
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DATE: 8/2/2011
DRAWN BY: C.F.S.
CHECKED BY: S.M.B. 8/4/2011
APPROVED BY: D.A.P. 8/4/2011
ST. FILE:
DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.



PAVEMENT MARKING PLAN
PAVEMENT REHABILITATION
FOR
GRISWOLD STREET
LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

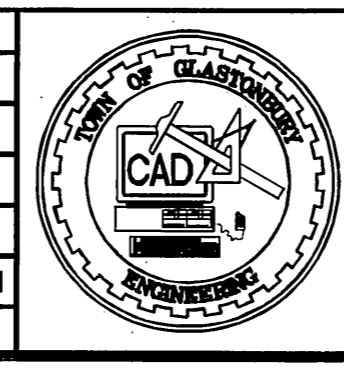
SHEET NO.
12
OF 15

FILE: H:\DWG\Streets\Griswold\STP\PW\14-Griswold-St-Pavement-Rehab-Griswold.dwg USER: Charles Steinhilber DATE: 11/29/2011

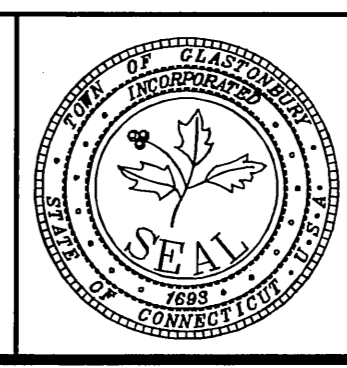
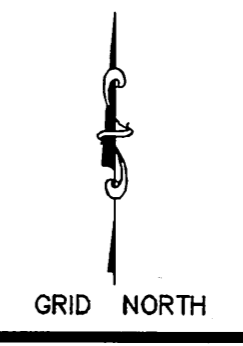


STP 53-184
PW-1003

DRAWING ISSUE STATUS		
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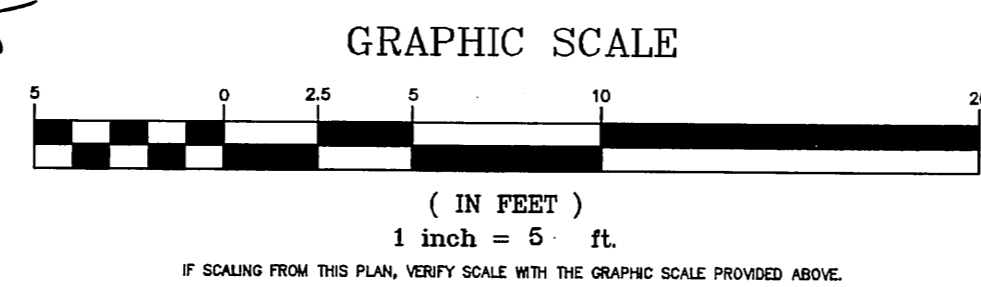
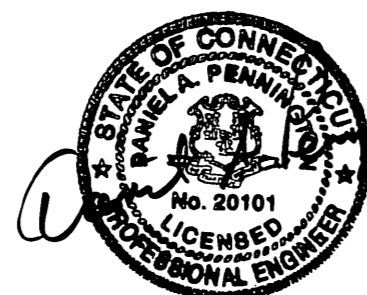
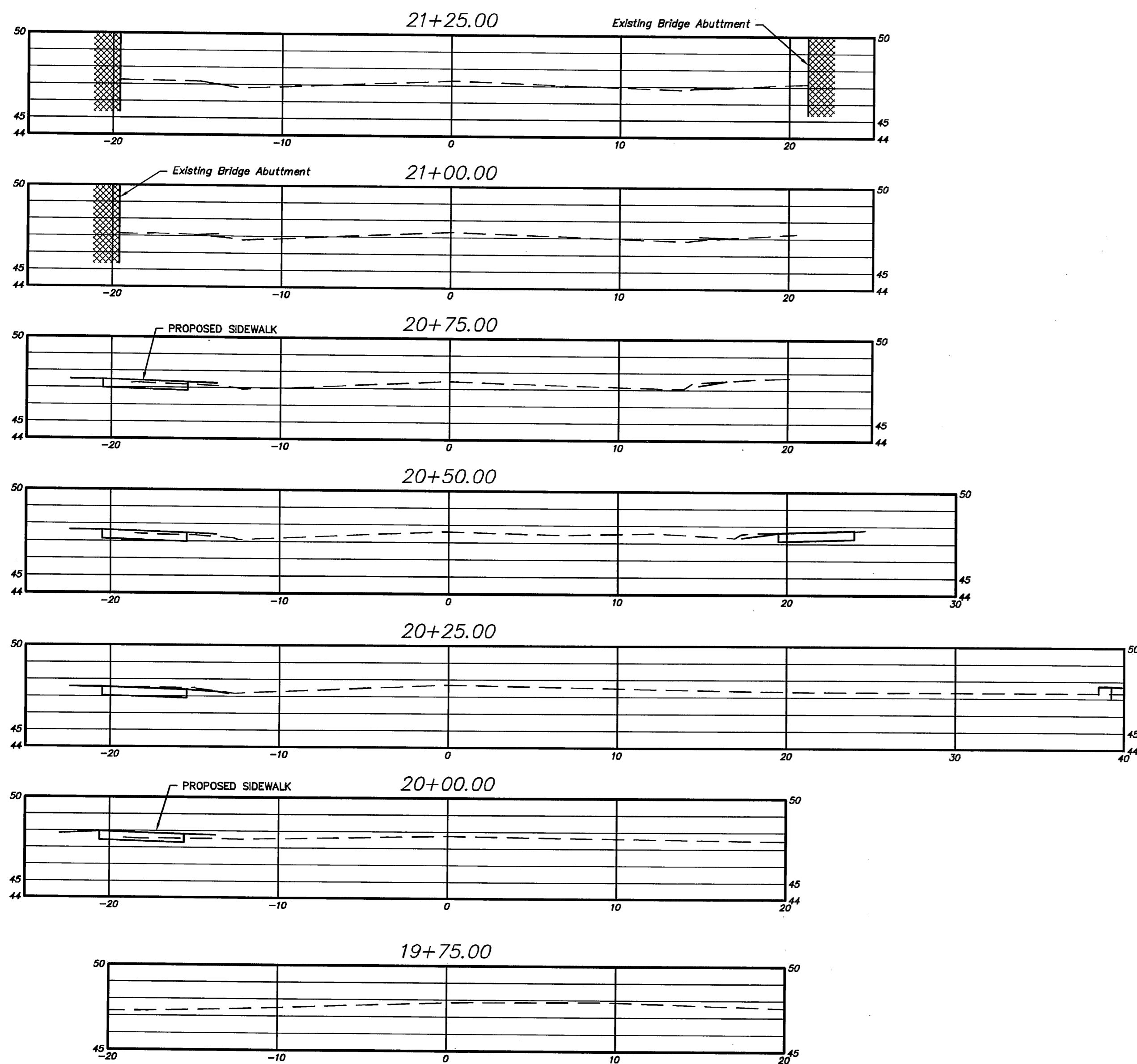
SCALE: AS SHOWN DATE: 1/11/2011
DRAWN BY: C.F.S.
CHECKED BY: S.M.B. 7/11/2011
APPROVED BY: D.A.P. 7/11/2011
ST. FILE:
DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.



CROSS - SECTIONS
PAVEMENT REHABILITATION
FOR
GRISWOLD STREET
LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

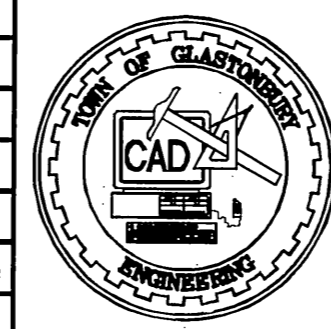
SHEET NO.
13
OF 15

FILE: H:\DWG\SS\Griswold\STP\1014-Griswold ST Pavement Rehab\Griswold ST Pavement Rehab - 10-2011.dwg USER: Charles Stashly DATE: 11/28/2011

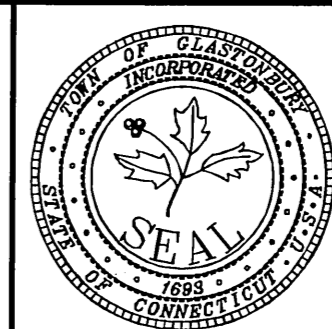
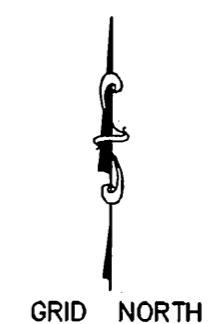


STP 53-184
PW-1003

DRAWING ISSUE STATUS		
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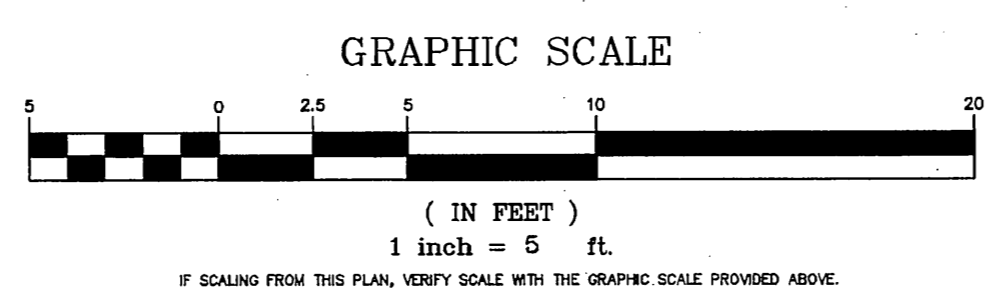
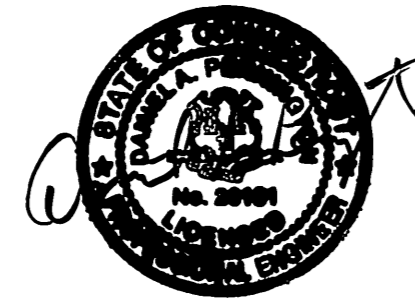
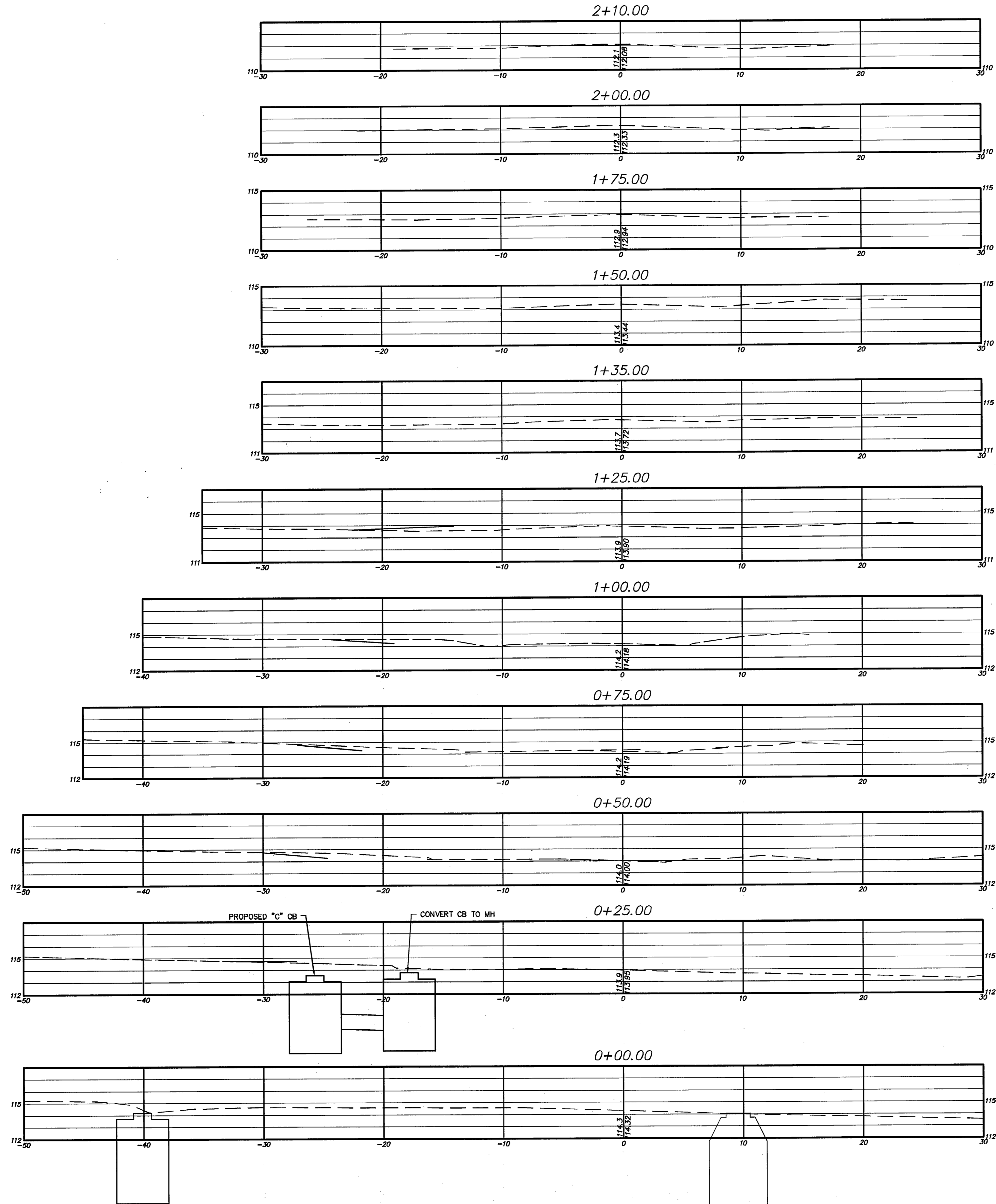
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DRAWN BY: C.F.S.	1/11/2011
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	
DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.	



CROSS - SECTIONS
PAVEMENT REHABILITATION
FOR
GRISWOLD STREET
LOCATED BETWEEN
MAIN STREET & CANDLEWOOD ROAD
GLASTONBURY, CONNECTICUT

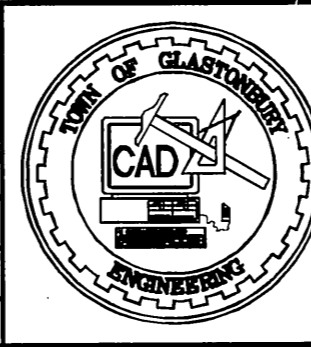
SHEET NO.
14
OF 15

FILE: J:\DWG\Streets\Griswold ST\104-Griswold ST Pavement Rehab\Griswold ST Pavement Rehab.dwg USER: Charles Steinhauser DATE: 11/23/2011

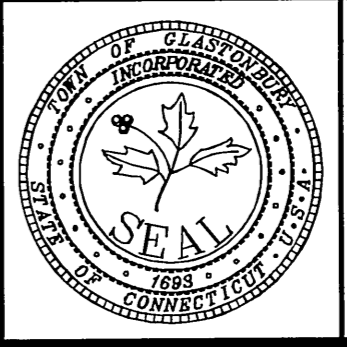
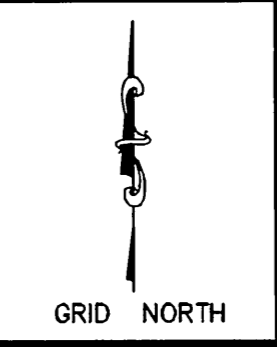


STP 53-184
PW-1003

DRAWING ISSUE STATUS		
NO.	DESCRIPTION	DATE
1.	ISSUED FOR CONSTRUCTION	11/23/2011

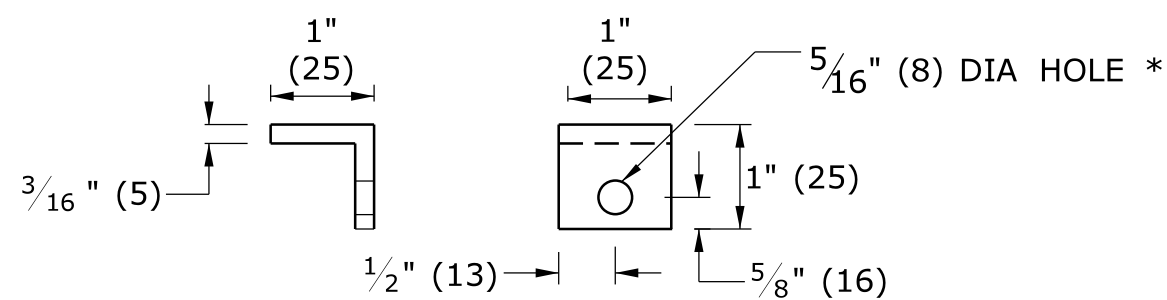


SCALE: AS SHOWN	DATE:
DRAWN BY: C.F.S.	1/11/2011
CHECKED BY: S.M.B.	7/11/2011
APPROVED BY: D.A.P.	7/11/2011
ST. FILE:	
DO NOT SCALE THIS DRAWING. USE THE DIMENSIONS GIVEN. IF THERE ARE ANY DISCREPANCIES OR QUESTIONS, CONTACT THE TOWN OF GLASTONBURY, ENGINEERING OFFICE.	



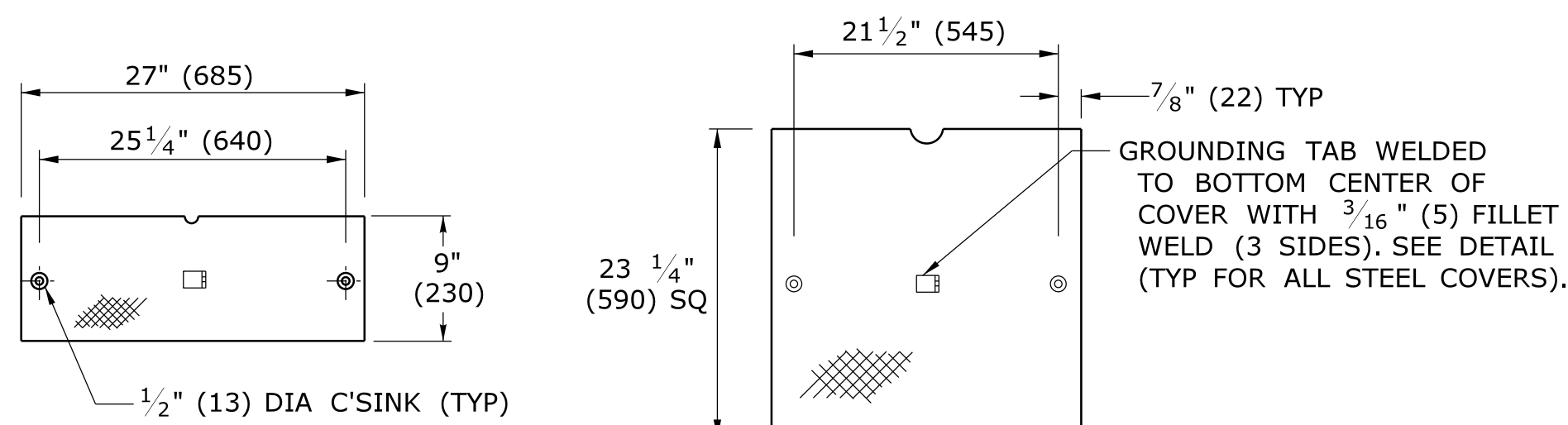
CROSS - SECTIONS
PAVEMENT REHABILITATION
FOR
CANDLEWOOD ROAD
LOCATED NORTH OF
GRISWOLD STREET
GLASTONBURY, CONNECTICUT

SHEET NO.
15
OF 15

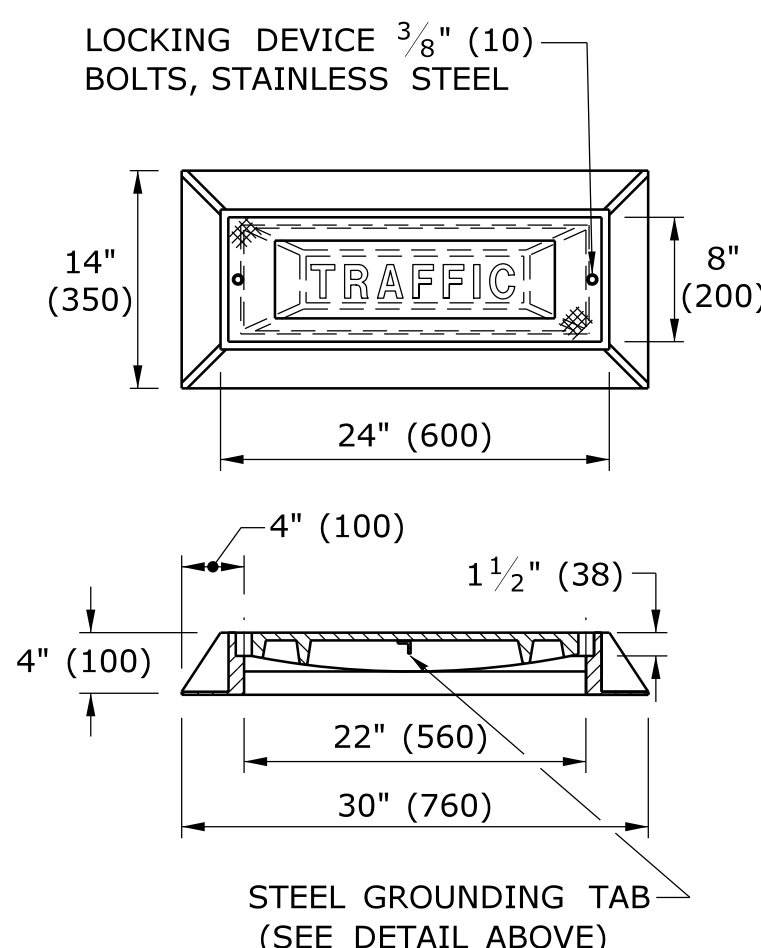


* ATTACH 6' (2 m) LENGTH OF NO. 8 GROUND WIRE TO GROUNDING TAB WITH ONE HOLE LUG, 1/4"-20 x 3/4" (M6 x 20) LG S&S HEX HEAD BOLT, AND S&S FLAT WASHER. ATTACH FREE END OF GROUND WIRE TO CONDUIT BONDING BUSHING IN HANDHOLE.

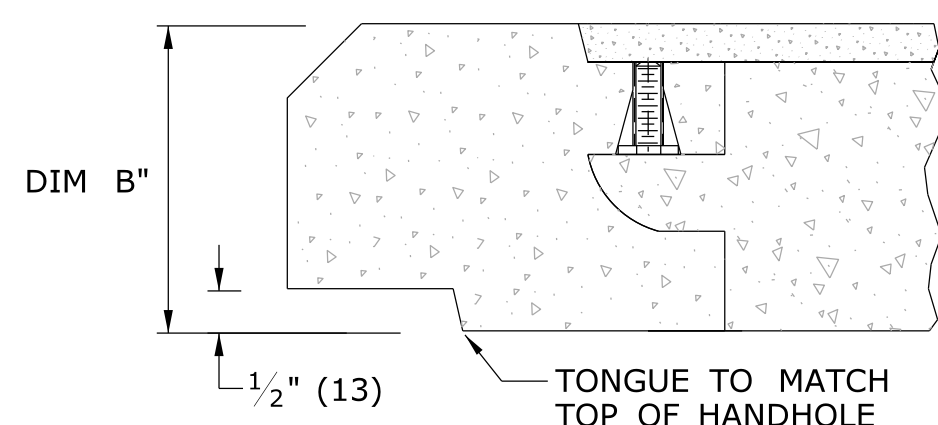
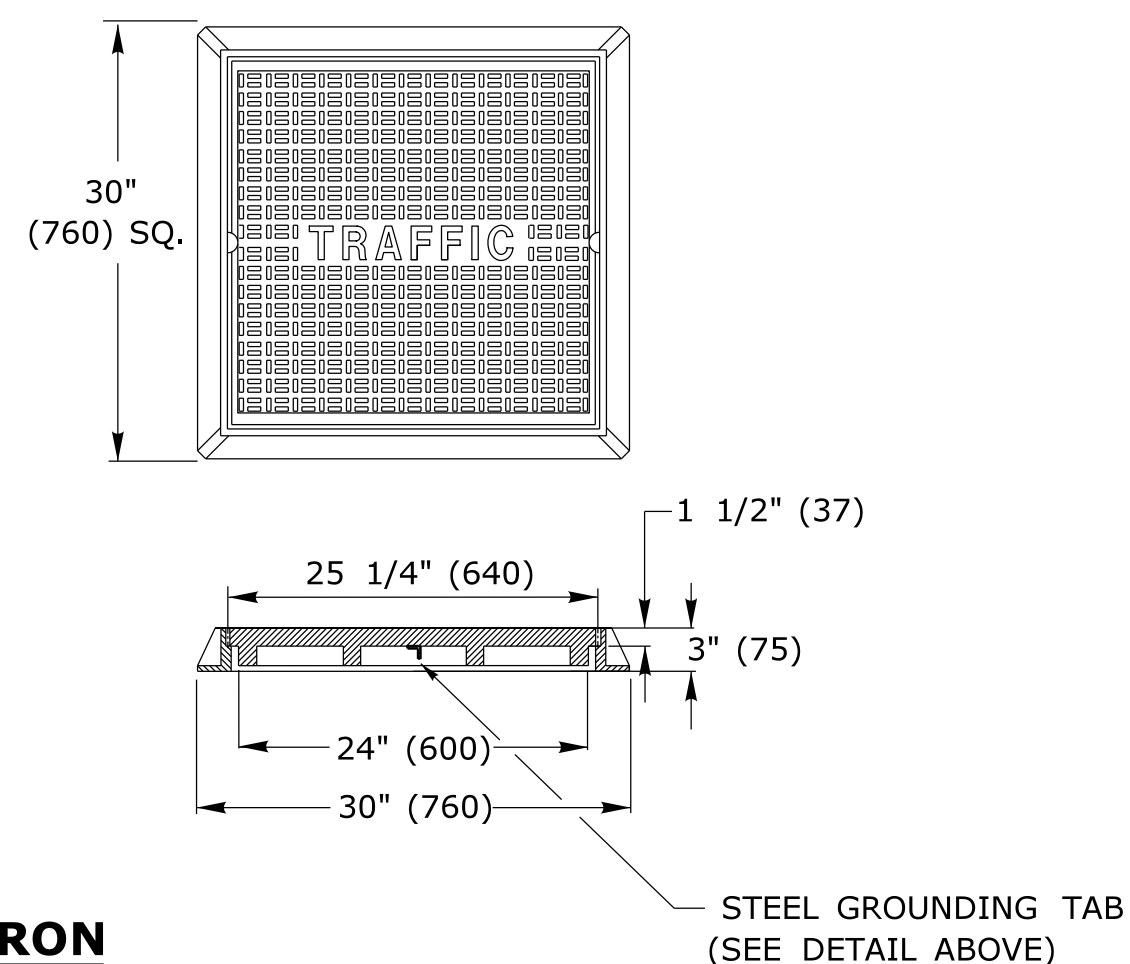
STEEL GROUNDING TAB



NON SKID FLOOR PLATE GALVANIZED STEEL, 3/8" (10)



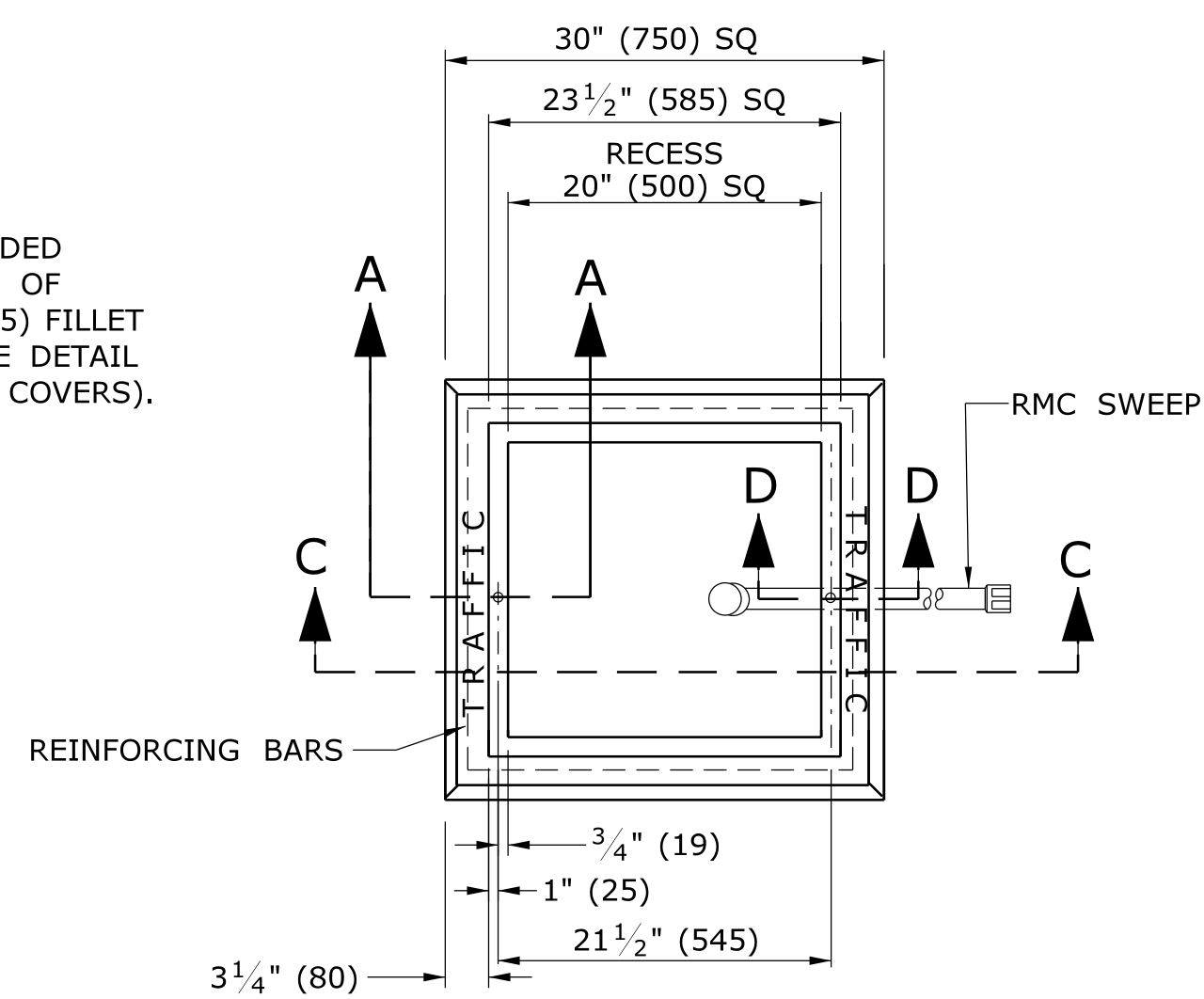
CAST IRON HANDHOLE COVERS



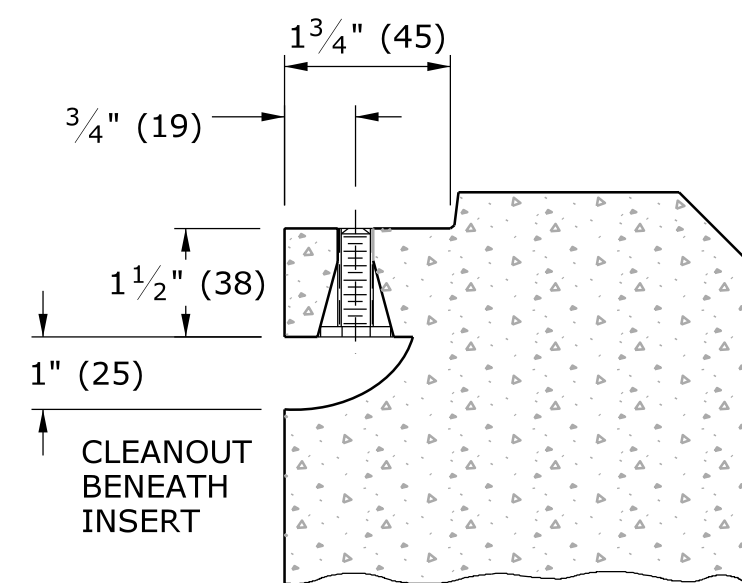
SECTION A-A HANDHOLE EXTENSIONS

12 - #8 REINFORCING BARS REQ'D

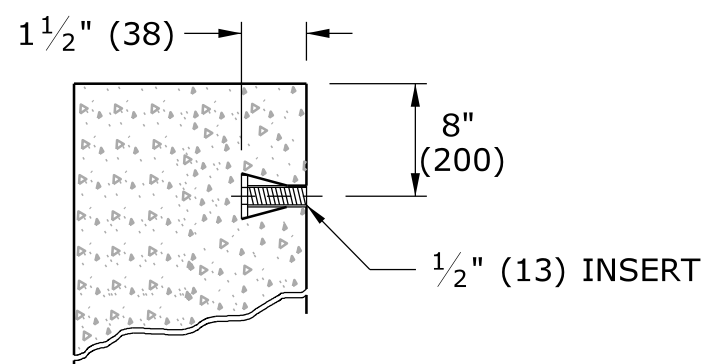
DIM. "B" CHART	ENGLISH HEIGHT	METRIC HEIGHT
2"	50	
4" SHOWN	100	
6"	150	
12"	300	



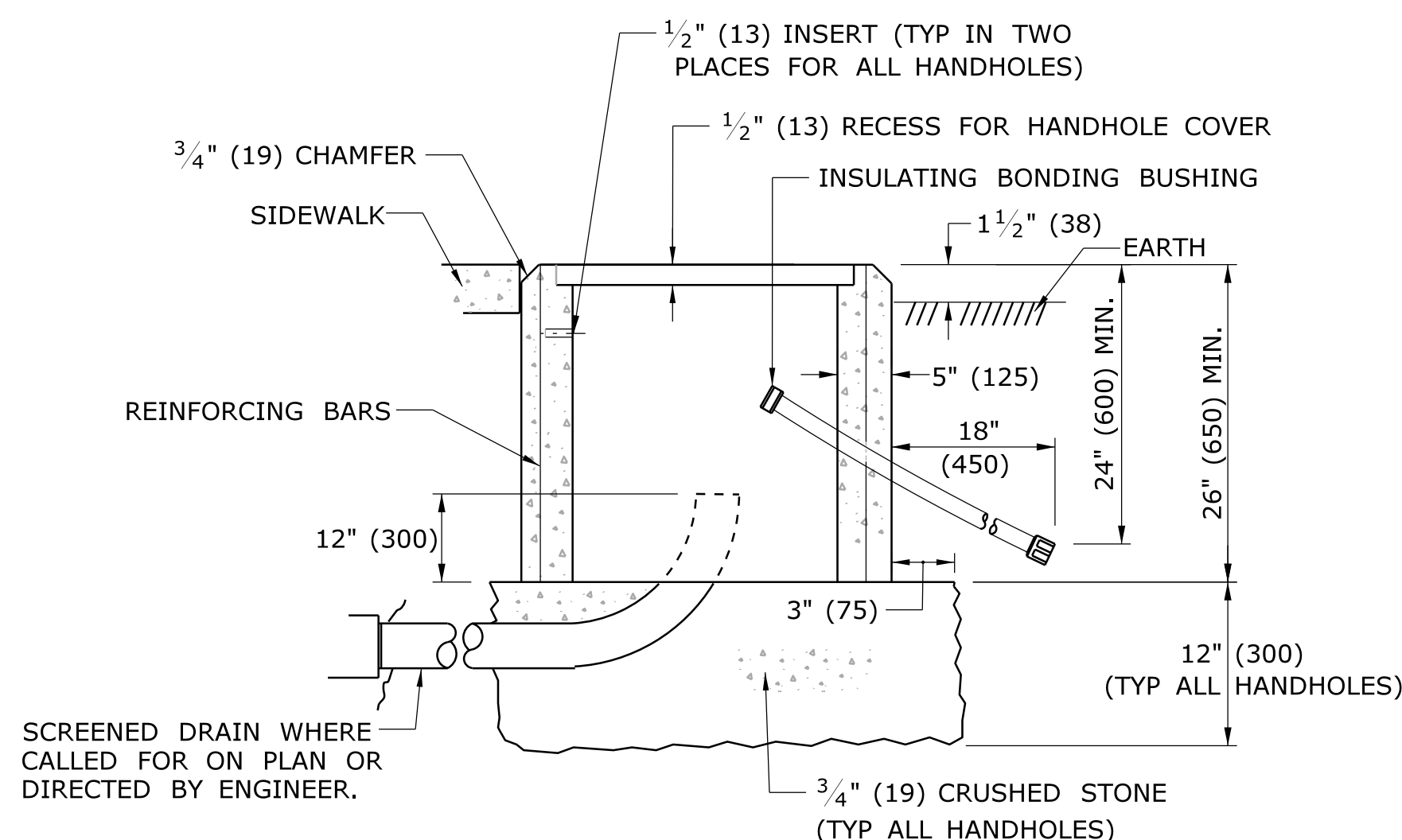
PLAN VIEW



SECTION D-D

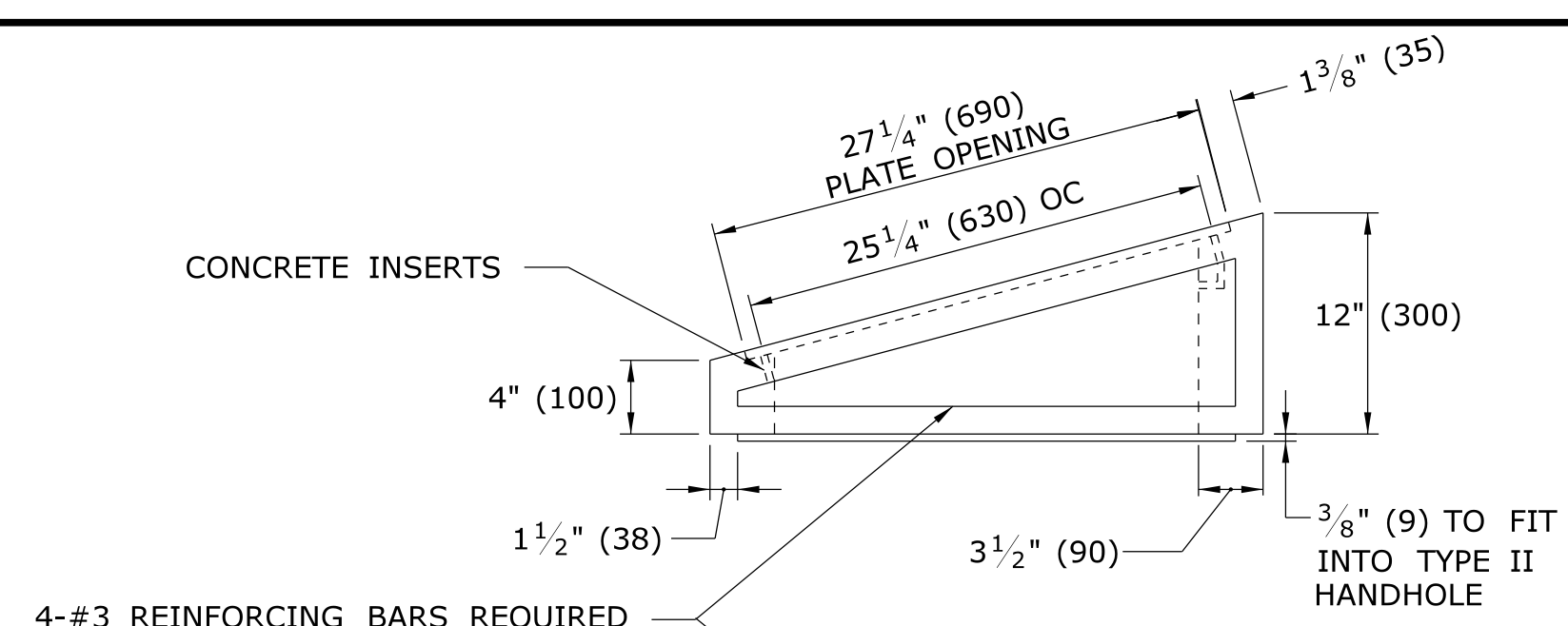


INSERT DETAIL



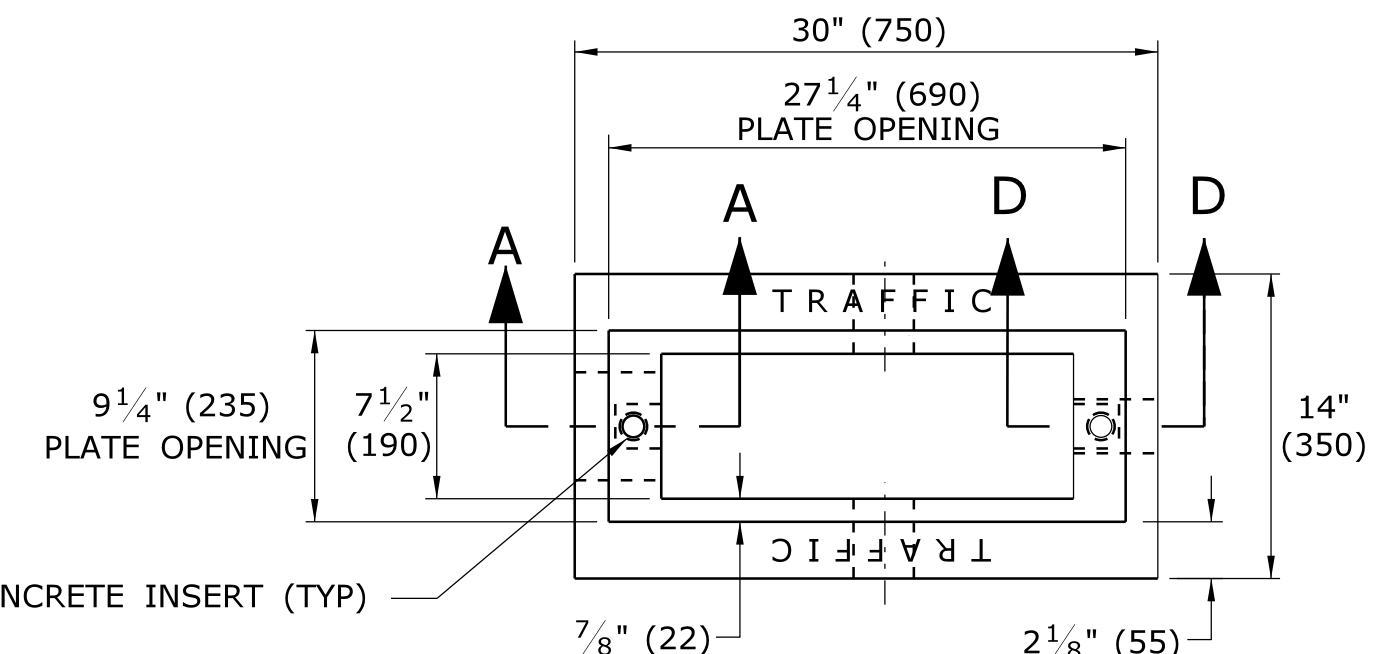
SECTION C-C

CONCRETE HANDHOLE CLASS "A" CONCRETE



TOP VIEW DIMENSIONS AND SECTION VIEWS SAME AS CONCRETE HANDHOLE.

CONCRETE HANDHOLE TYPE II BANK ADAPTER



CONCRETE HANDHOLE TYPE II CLASS "C" CONCRETE

NOTES:

- BLOCK UNUSED OPENINGS OF HANDHOLE ON THE OUTSIDE WITH PRESSURE TREATED PLYWOOD.
- GROUT AROUND ALL CONDUITS.
- USE 1 1/2" x 3/8" (38 x 10) CONCRETE INSERT. STANDARD THREAD, STAINLESS STEEL, FLAT HEAD BOLT, RECESSED IN PLATE COVER. INSERTS TO HAVE CLEANOUTS.
- TYPE II HANDHOLE 30" (760) SIDE INSTALLED PARALLEL TO ROAD UNLESS OTHERWISE NOTED.
- CAST THE WORD "TRAFFIC" INTO TOP EDGE OF HANDHOLE, 1 1/2" (38) LETTERS.
- WHERE AN EXISTING CONCRETE SIDEWALK SLAB ABUTTING A HANDHOLE IS DAMAGED OR CUT DURING INSTALLATION, THE ENTIRE SECTION SHALL BE REPLACED.
- 12-#3 REINFORCING BARS REQUIRED FOR ALL HANDHOLES. (8 HORIZONTAL, 4 VERTICAL)

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:
 □ PROPOSED HANDHOLE
 ■ EXISTING HANDHOLE

REV.	DATE	REVISION DESCRIPTION
-		

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Plotted Date: 9/11/2009

NOT TO SCALE

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1010_01

SUBMITTED BY: NAME/DATE/TIME:

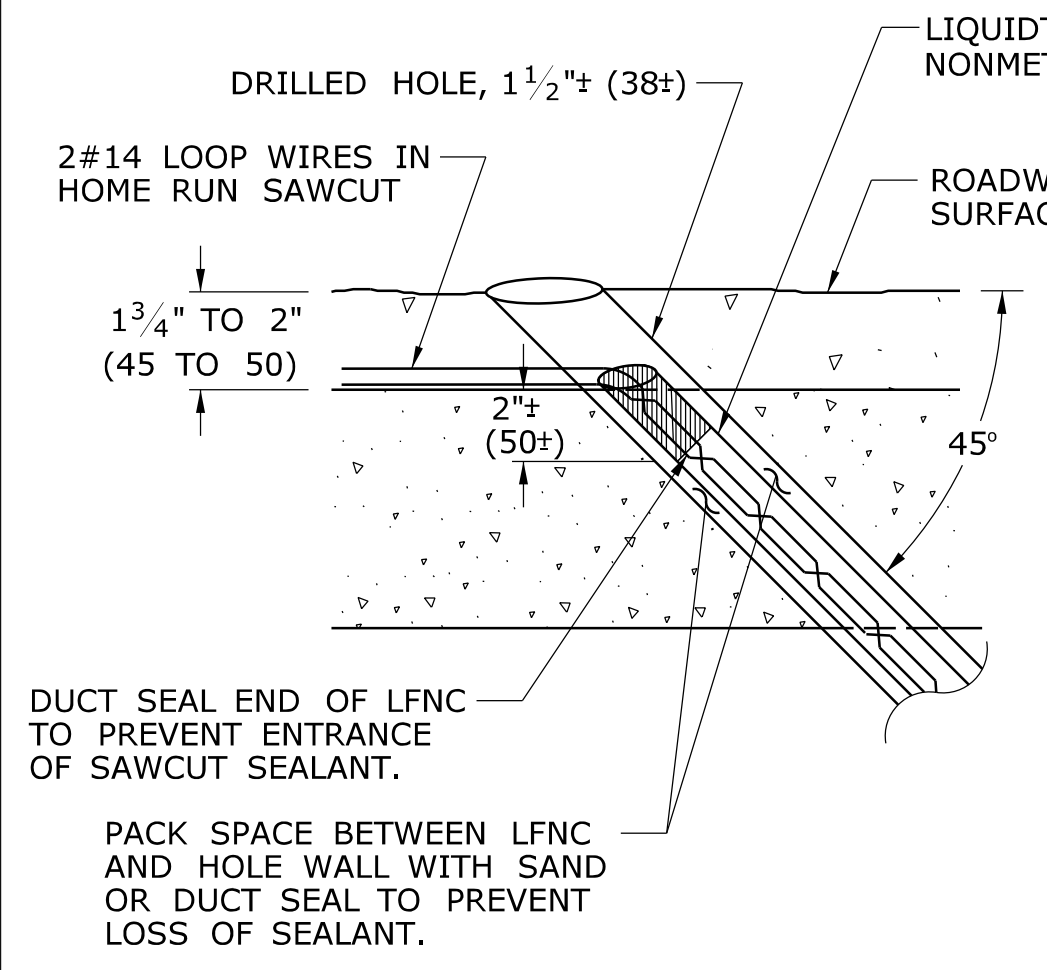
APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

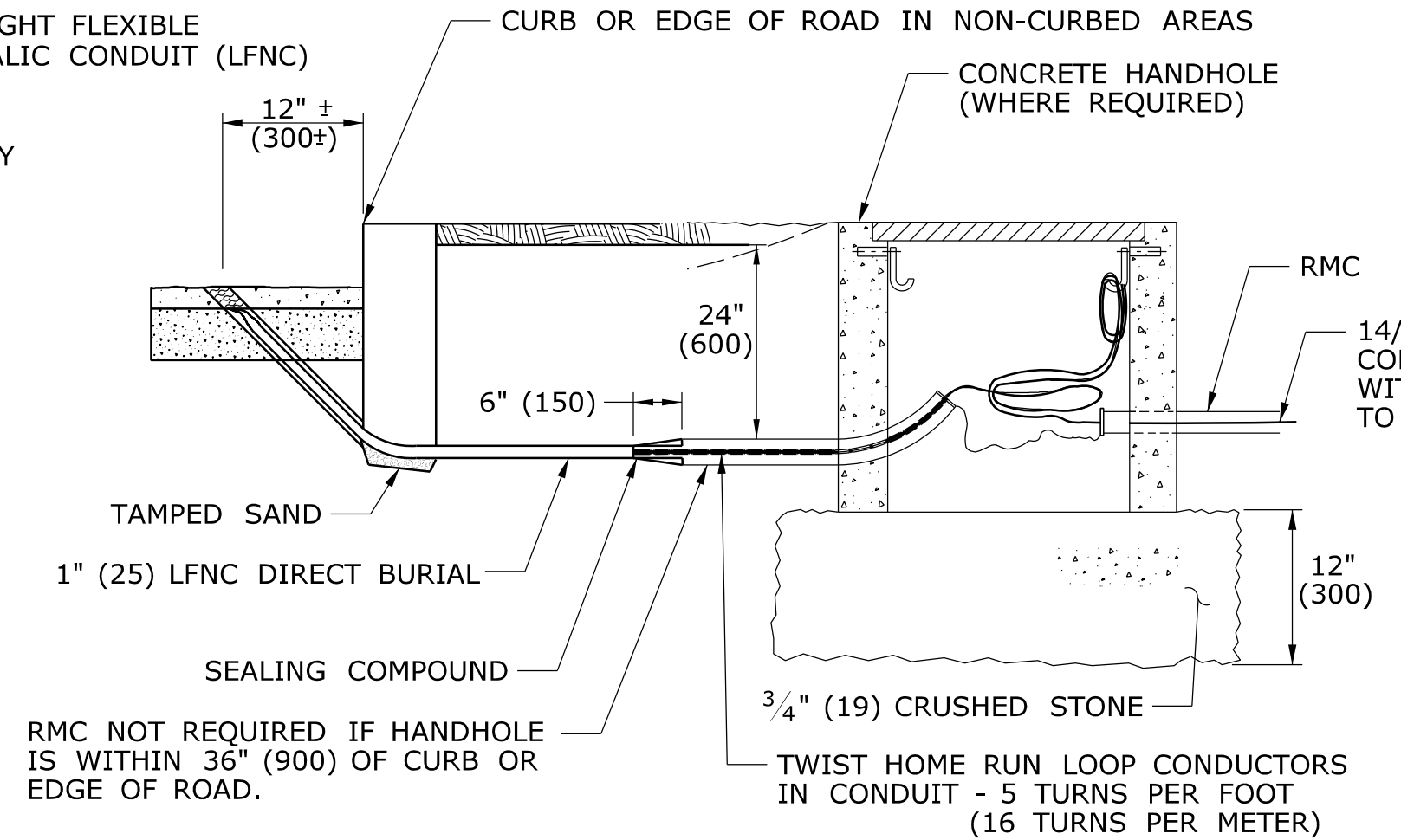
OFFICE OF ENGINEERING

STANDARD SHEET TITLE: CONCRETE HANDHOLE

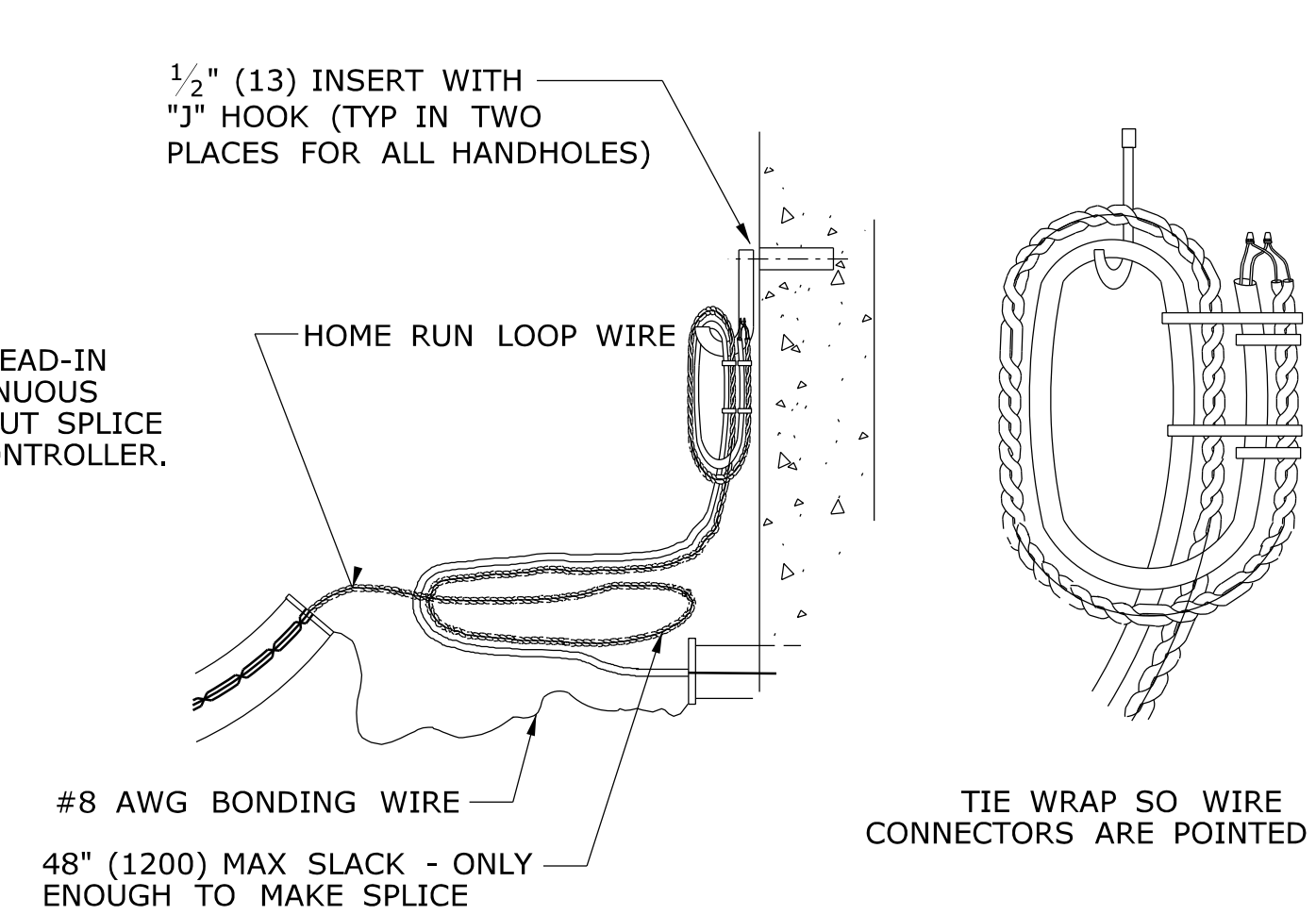
STANDARD SHEET NO.: TR-1010_01



DETAIL "A"

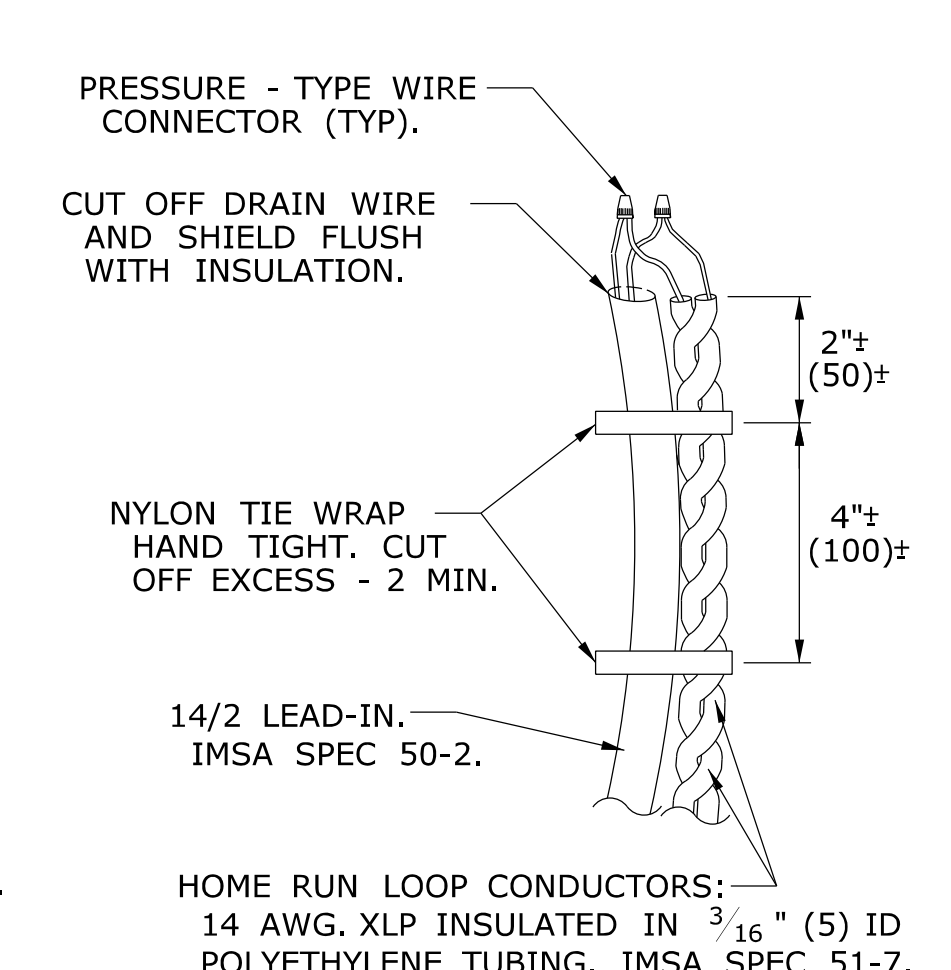


**TYPICAL ELEVATION VIEW
LOOP DETECTOR SAWCUT AND LEAD-IN**

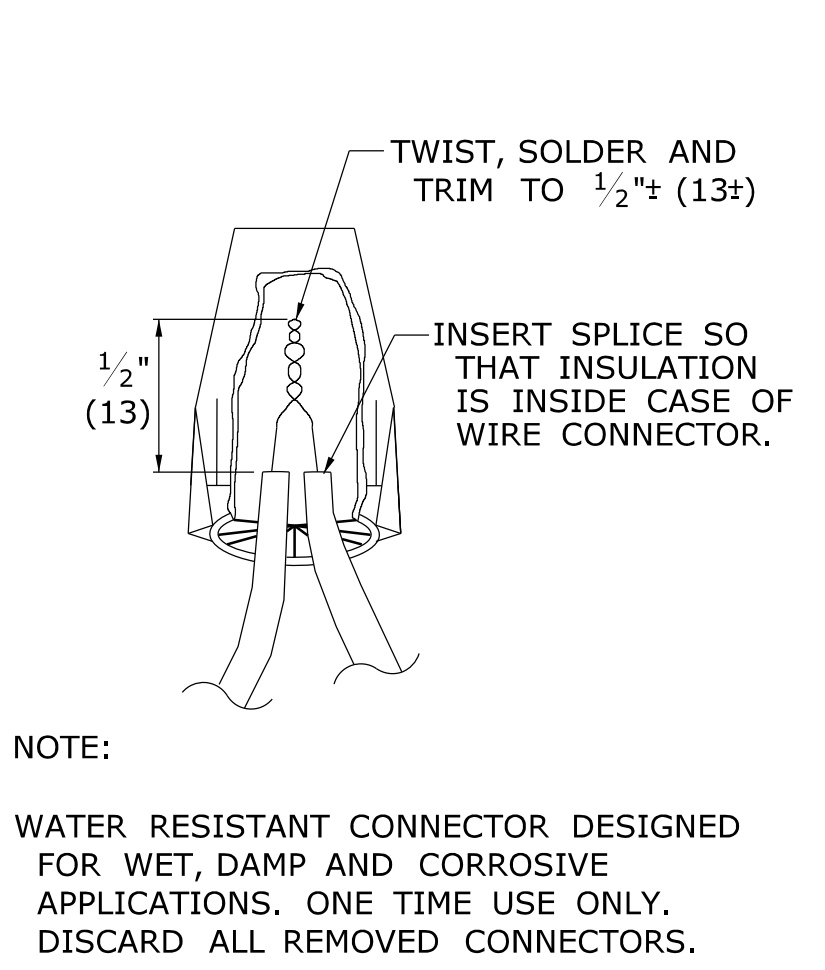


DETAIL "B"

**DETAIL "C"
FRONT VIEW**

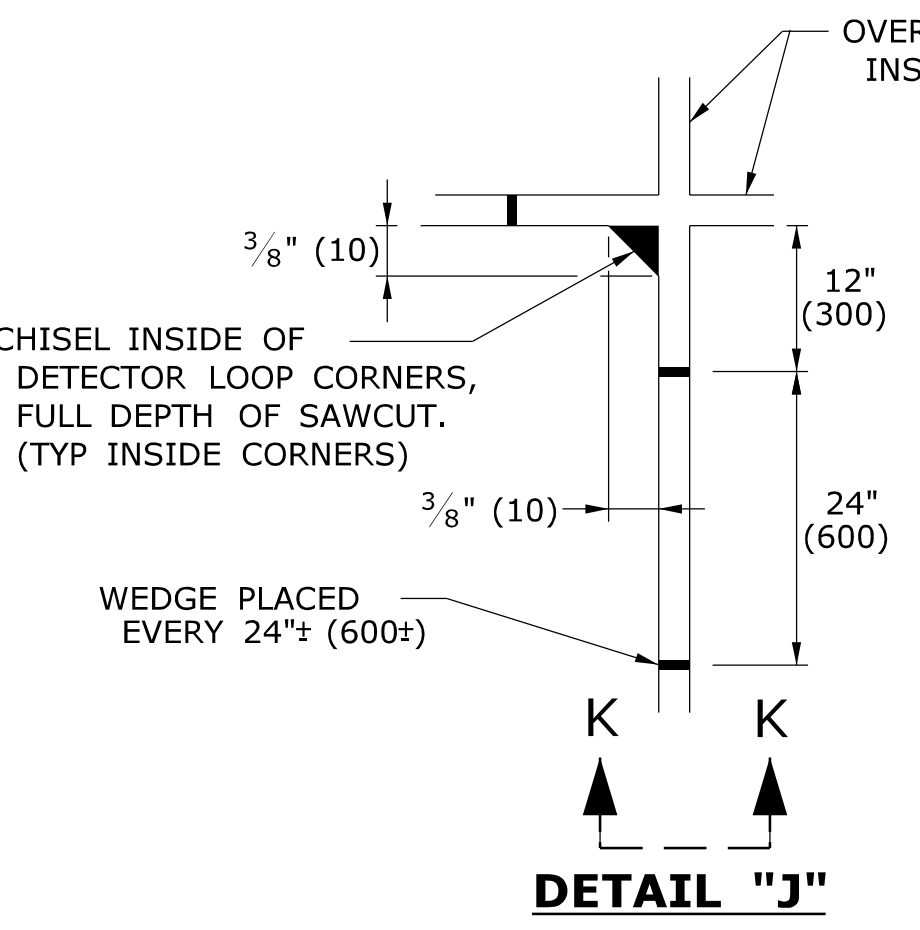
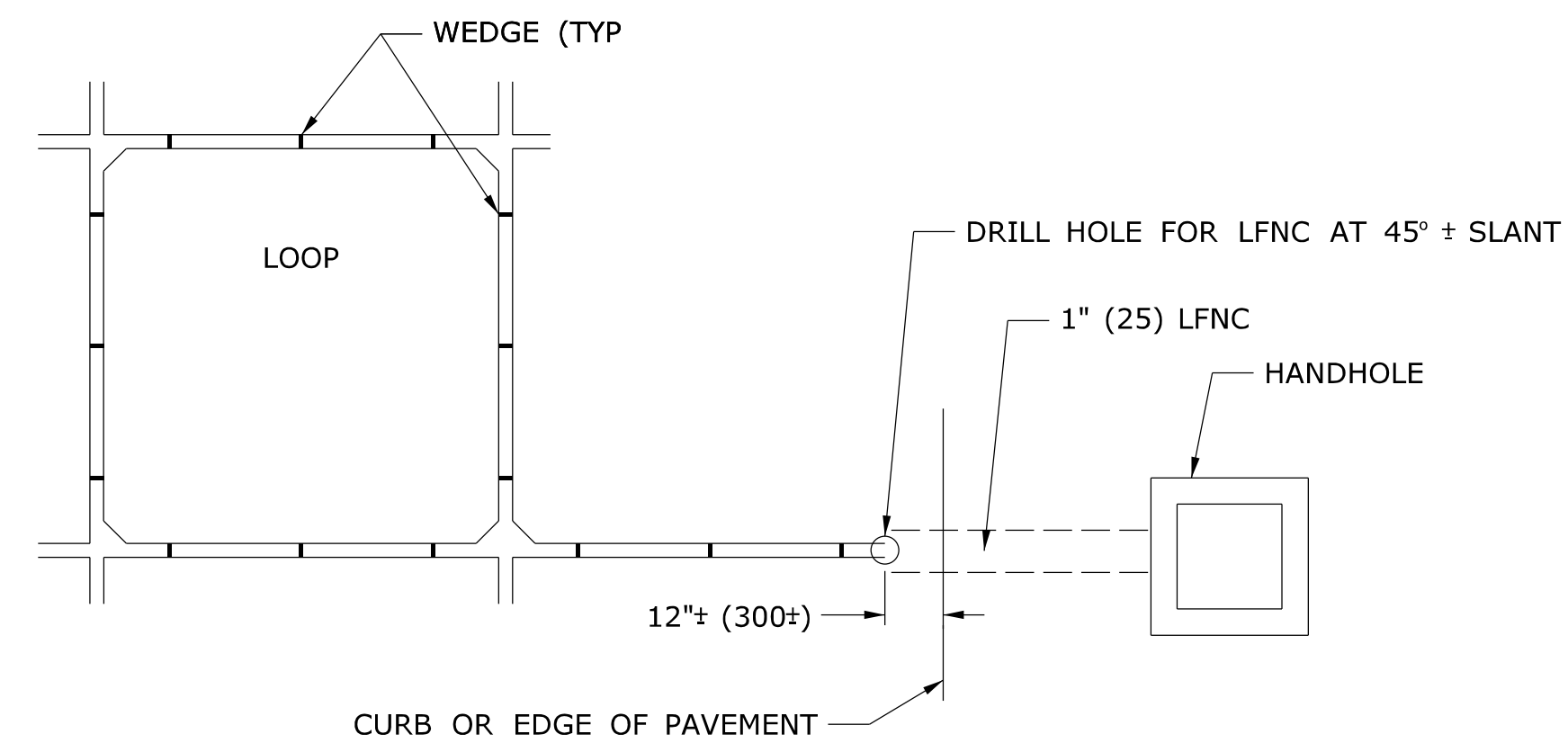


DETAIL "D"

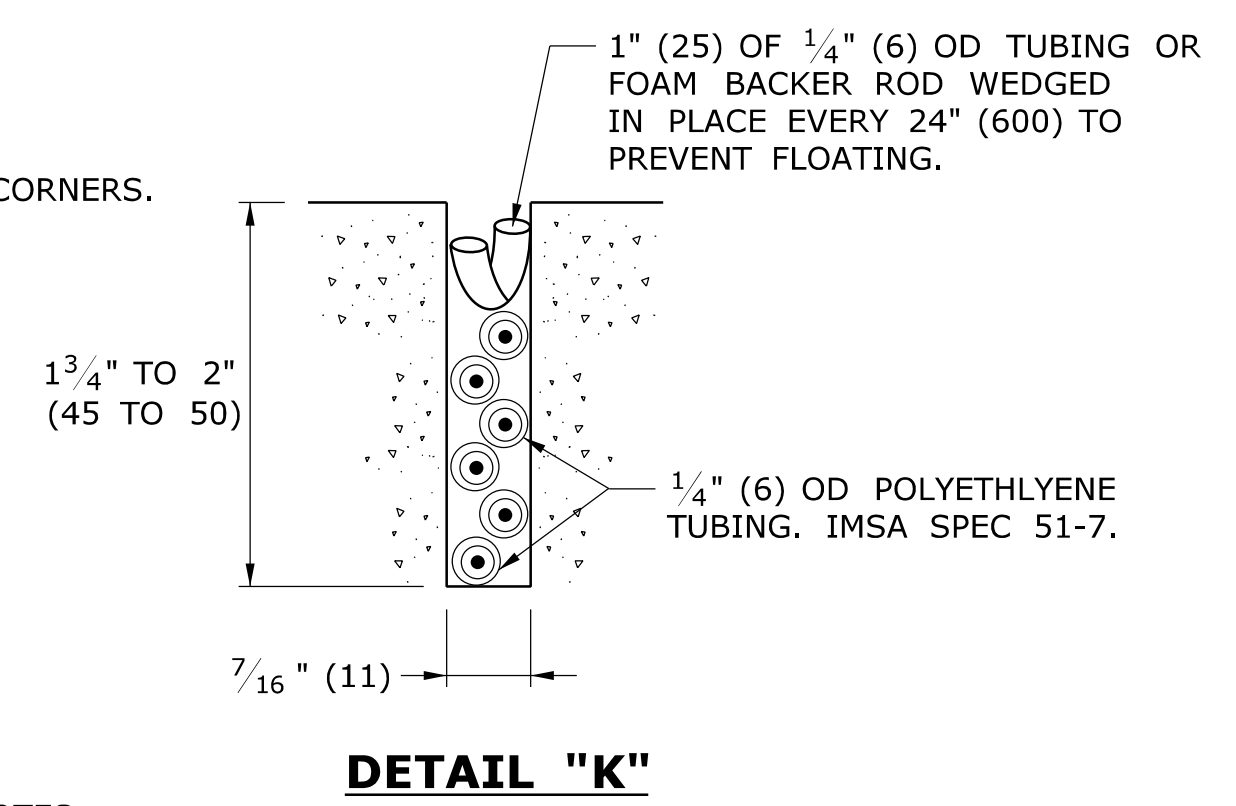


DETAIL "E"

NOTE:
WATER RESISTANT CONNECTOR DESIGNED FOR WET, DAMP AND CORROSIVE APPLICATIONS. ONE TIME USE ONLY. DISCARD ALL REMOVED CONNECTORS.

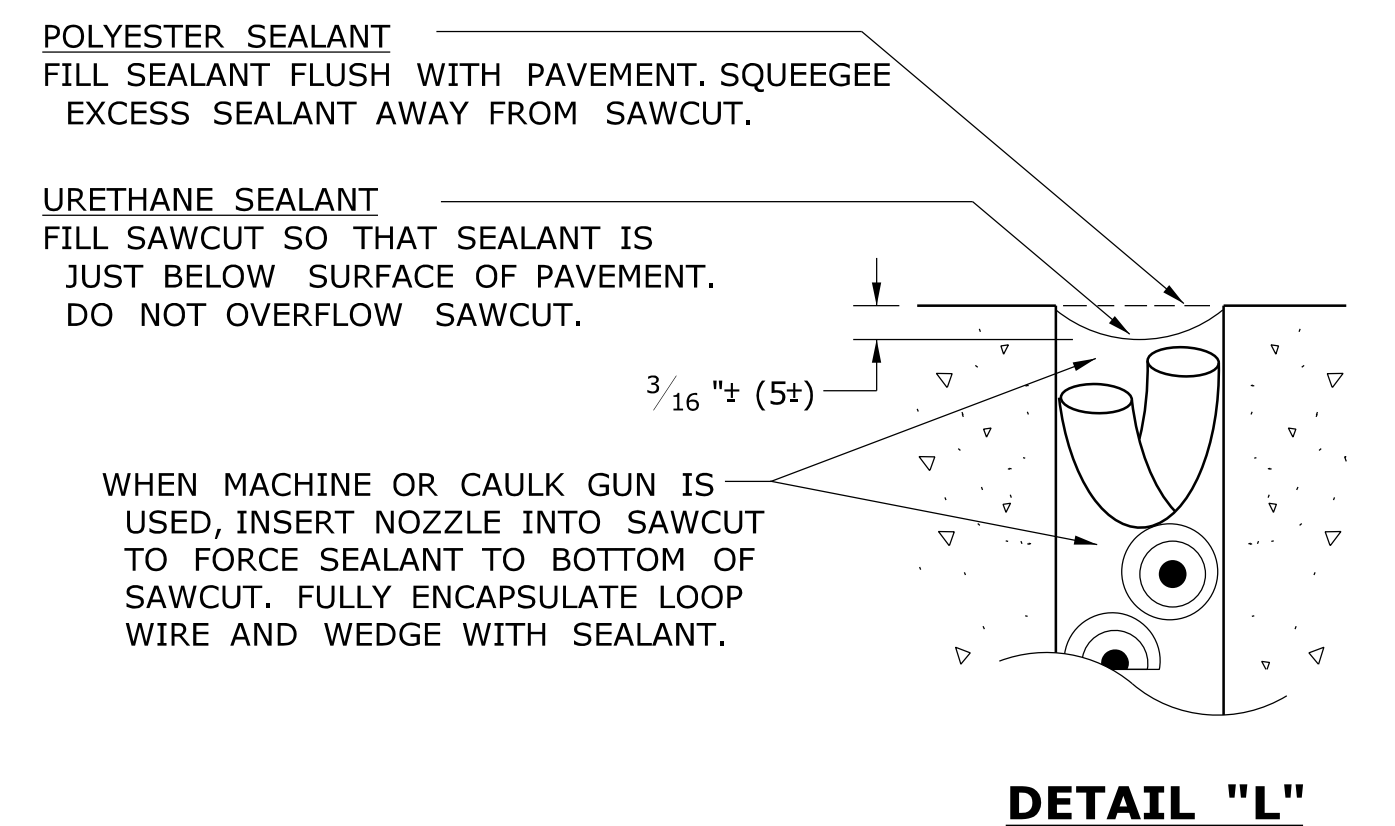


DETAIL "J"



DETAIL "K"

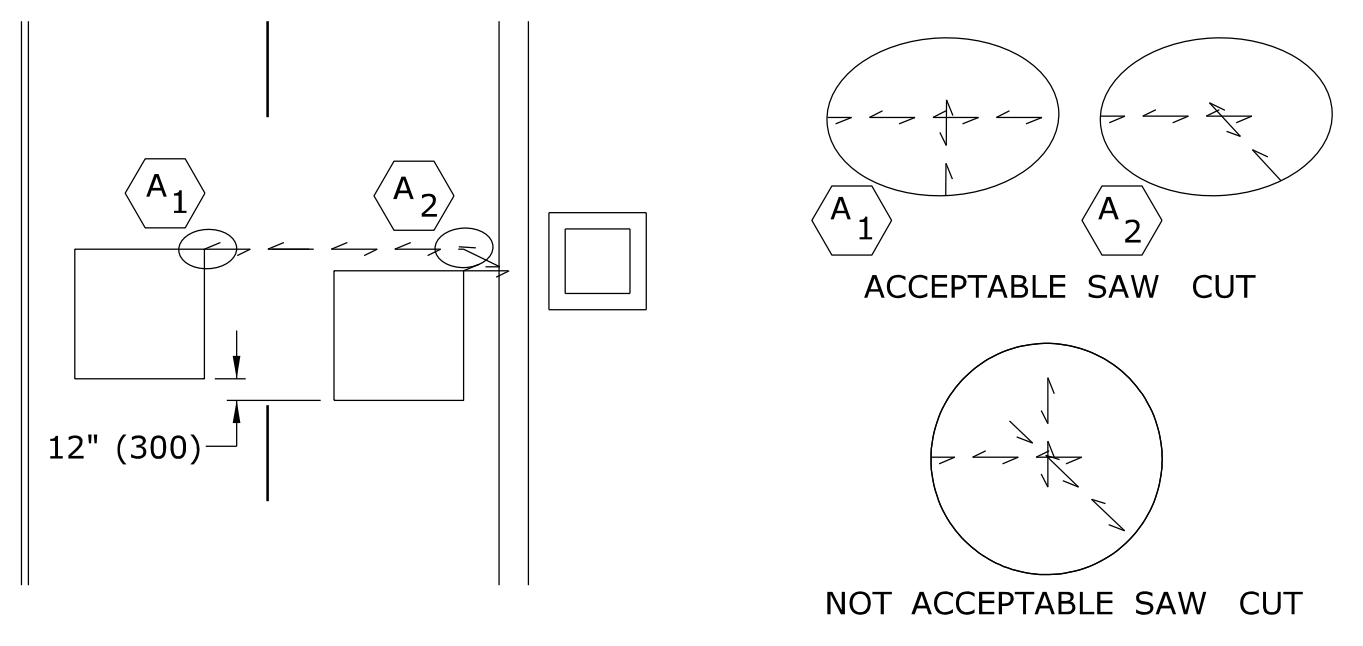
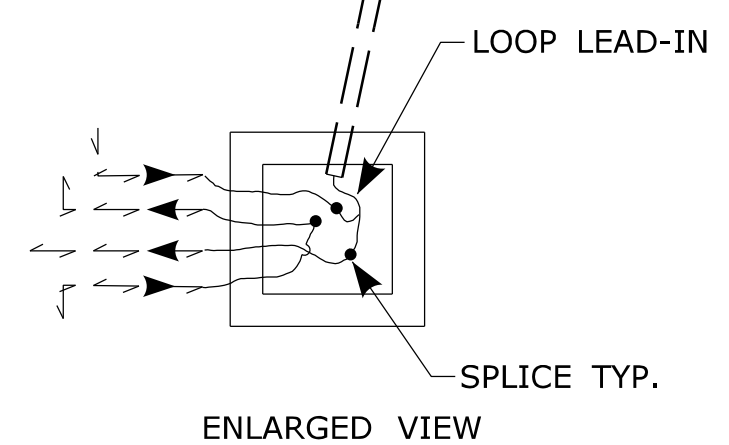
NOTES:
ONLY USE POLYESTER COMPOUND AS SEALANT, UNLESS OTHER TYPE IS APPROVED BY ENGINEER. REFER TO STANDARD SPECIFICATIONS, SECTION 11-11.
WET SAW UNLESS DRY SAW IS APPROVED BY ENGINEER.
RECOMMENDED SAW BLADE: 14" x 3/8" (350 x 10) PRODUCES 7/16" (11) SLOT.



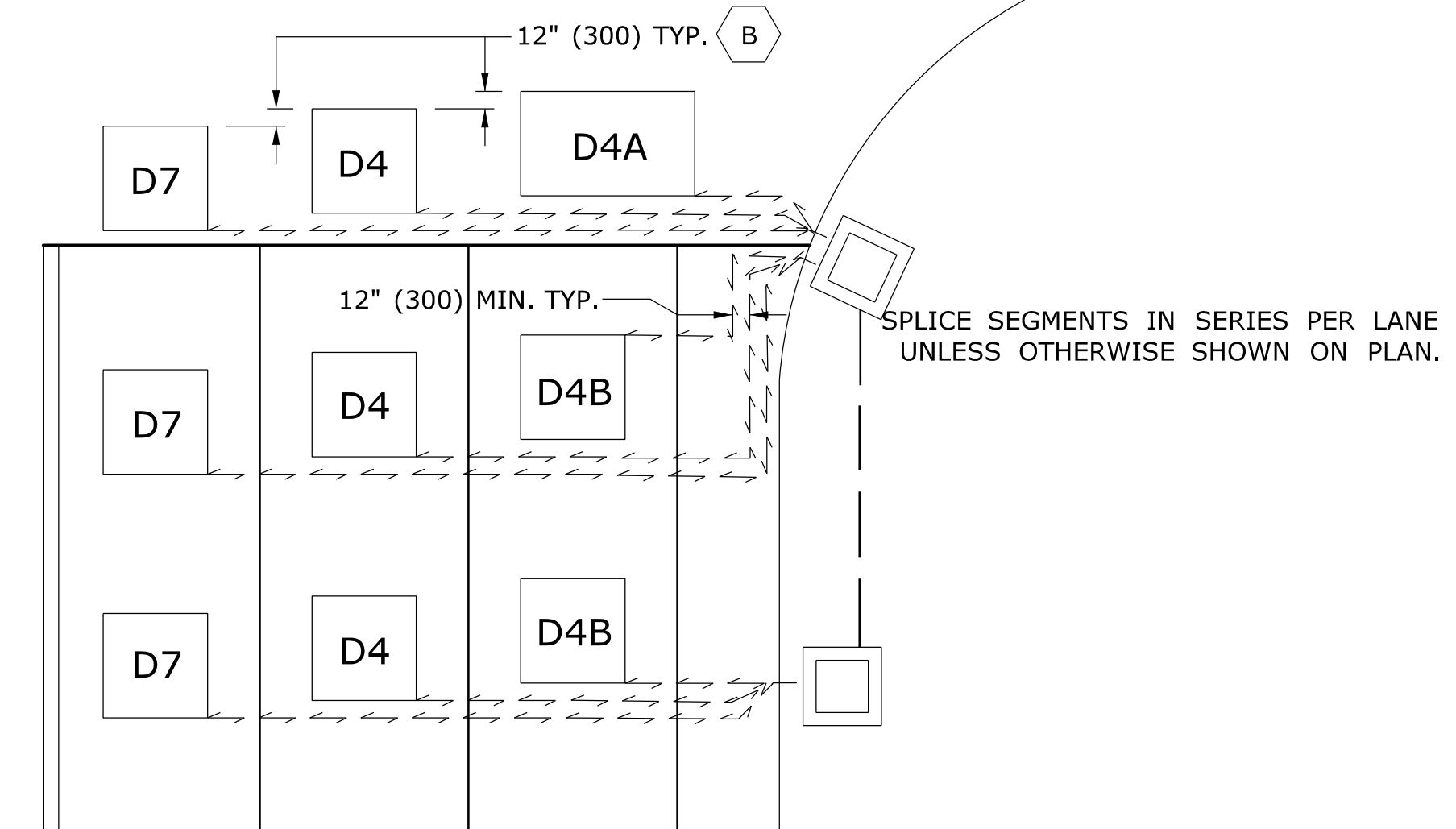
DETAIL "L"

TYPICAL WINDING SEGMENTED LOOPS, 3 TURNS EACH

NOTE:
TO CREATE A UNIFORM MAGNETIC FIELD, WIND ADJACENT LOOPS IN OPPOSITE DIRECTIONS.



A DO NOT OVERLAP MORE THAN TWO SAWCUTS.



B OFFSET ADJACENT INSIDE LANE(S) LOOP SEGMENTS 12" (300) +/- SO THAT SAWCUT FROM CORNER OF LOOP TO CURB IS STRAIGHT. LOOP NUMBERS AND PLACEMENT ARE FOR EXAMPLE ONLY.

LEGEND AS SHOWN ON TRAFFIC CONTROL SIGNAL PLAN:

	INDUCTIVE LOOP DETECTOR
	SAW CUT
	RIGID METAL CONDUIT
	HANDHOLE

REV.	DATE	REVISION DESCRIPTION

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NOT TO SCALE

Plotted Date: 10/30/2009

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

File name: CTDOT-TRAFFIC-STD.dgn Model: TR-1111_01

SUBMITTED BY:	NAME/DATE/TIME:
APPROVED BY:	NAME/DATE/TIME:

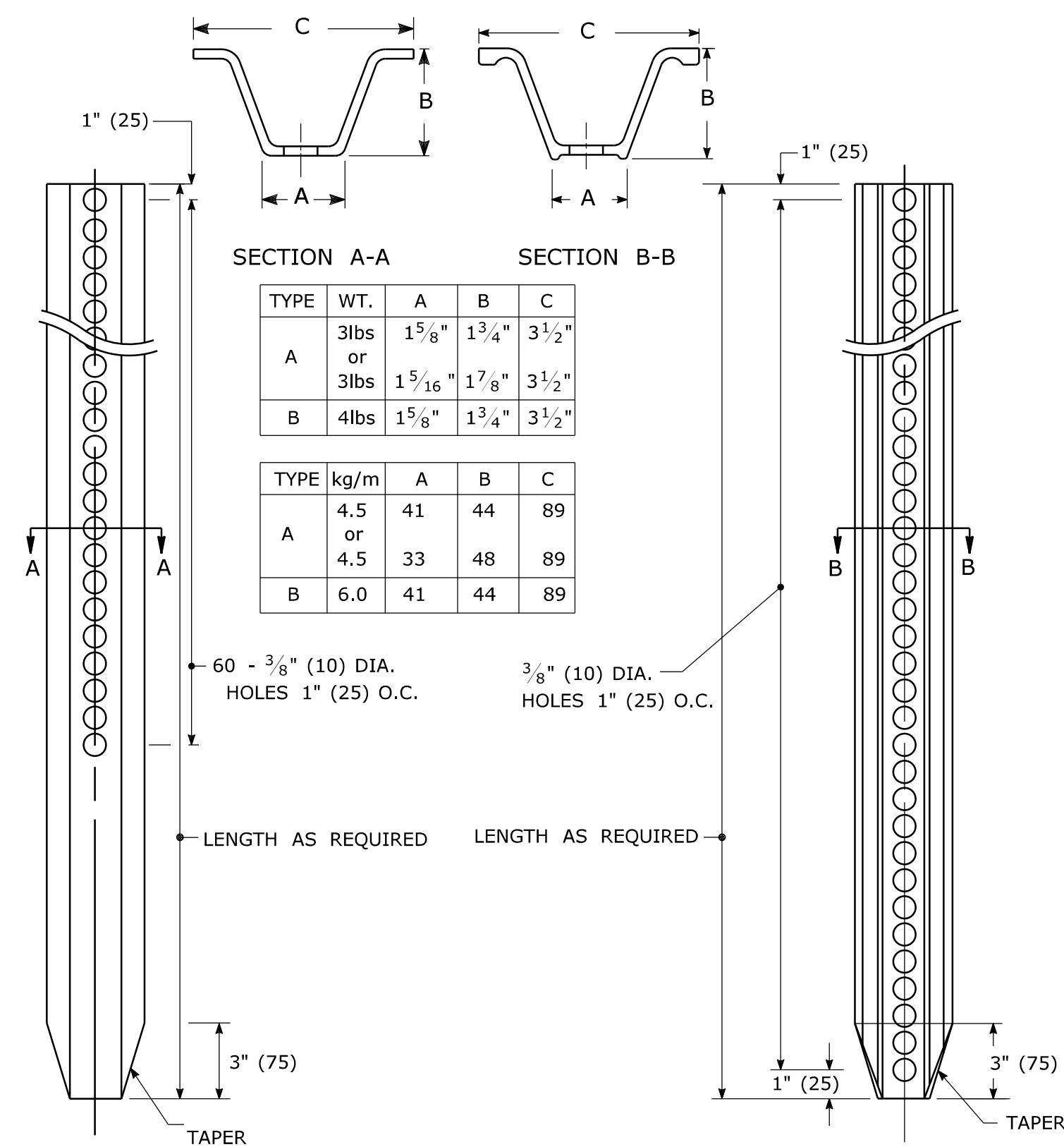
**CTDOT
STANDARD SHEET**

OFFICE OF ENGINEERING

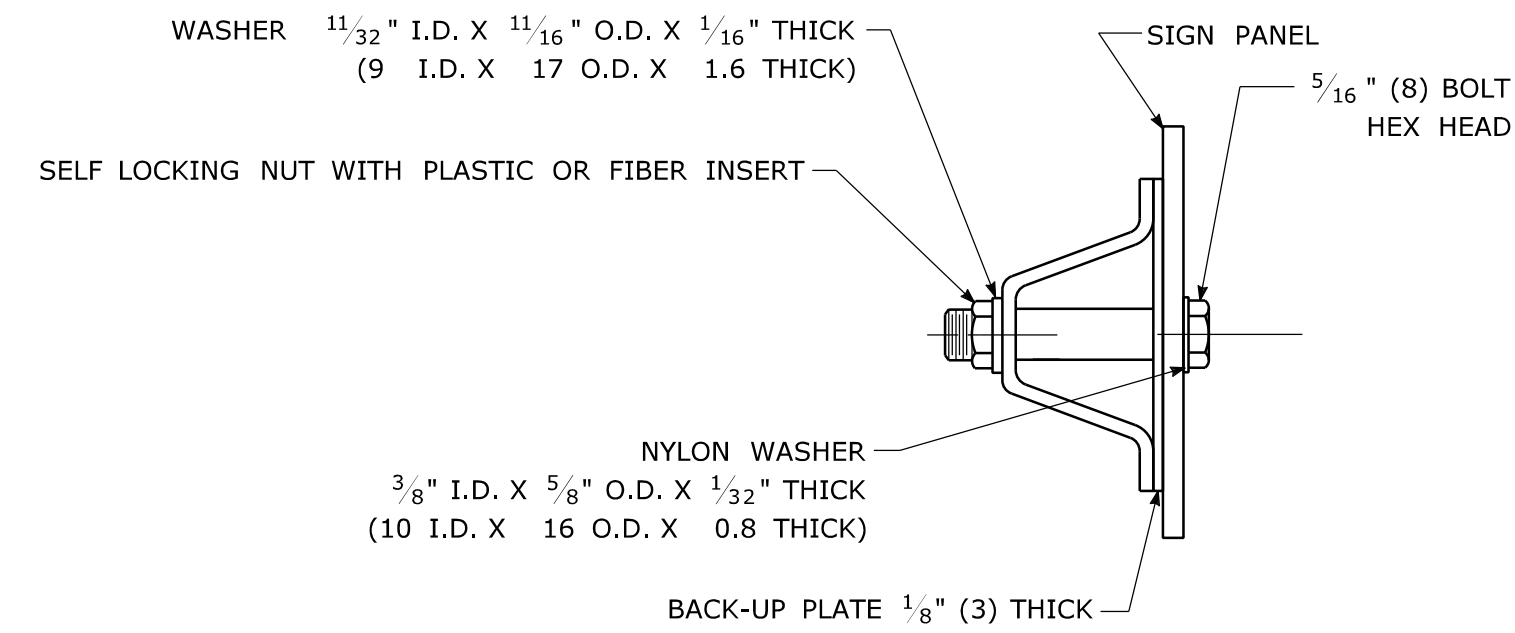
STANDARD SHEET TITLE:
**LOOP VEHICLE DETECTOR
AND SAWCUT**

STANDARD SHEET NO.:
TR-1111_01

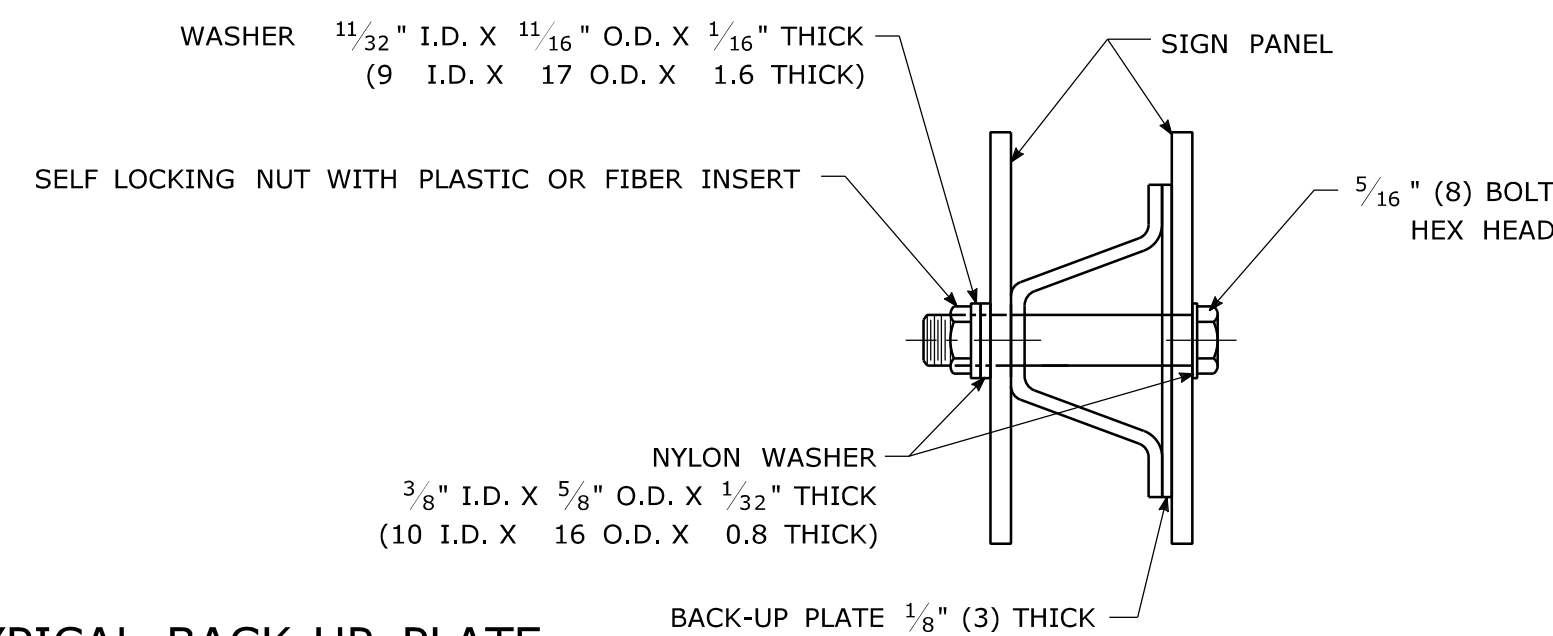
TYPICAL METAL SIGN POSTS



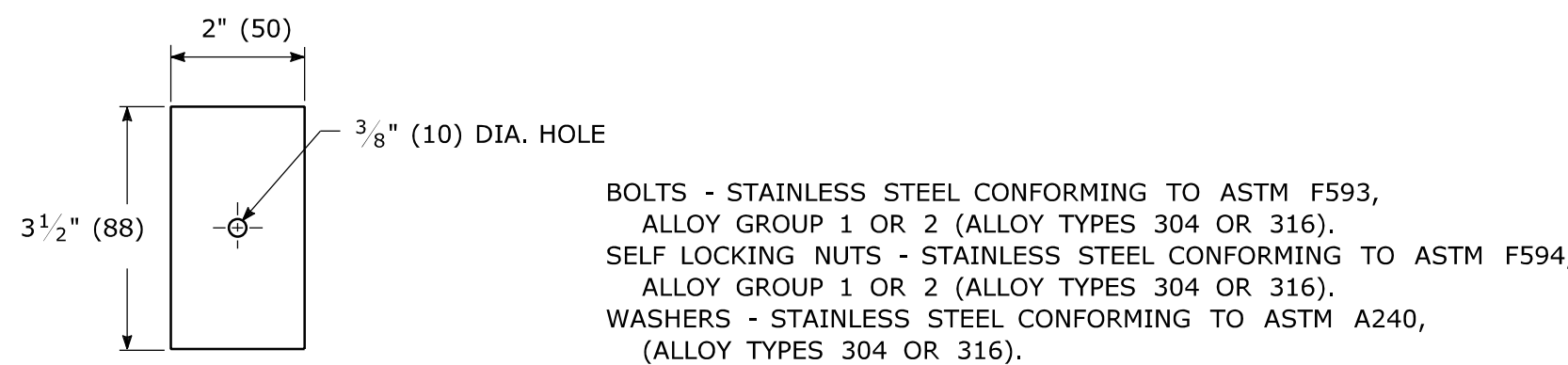
TYPICAL SIGN PANEL ATTACHMENT



TYPICAL BACK TO BACK SIGN PANEL ATTACHMENT

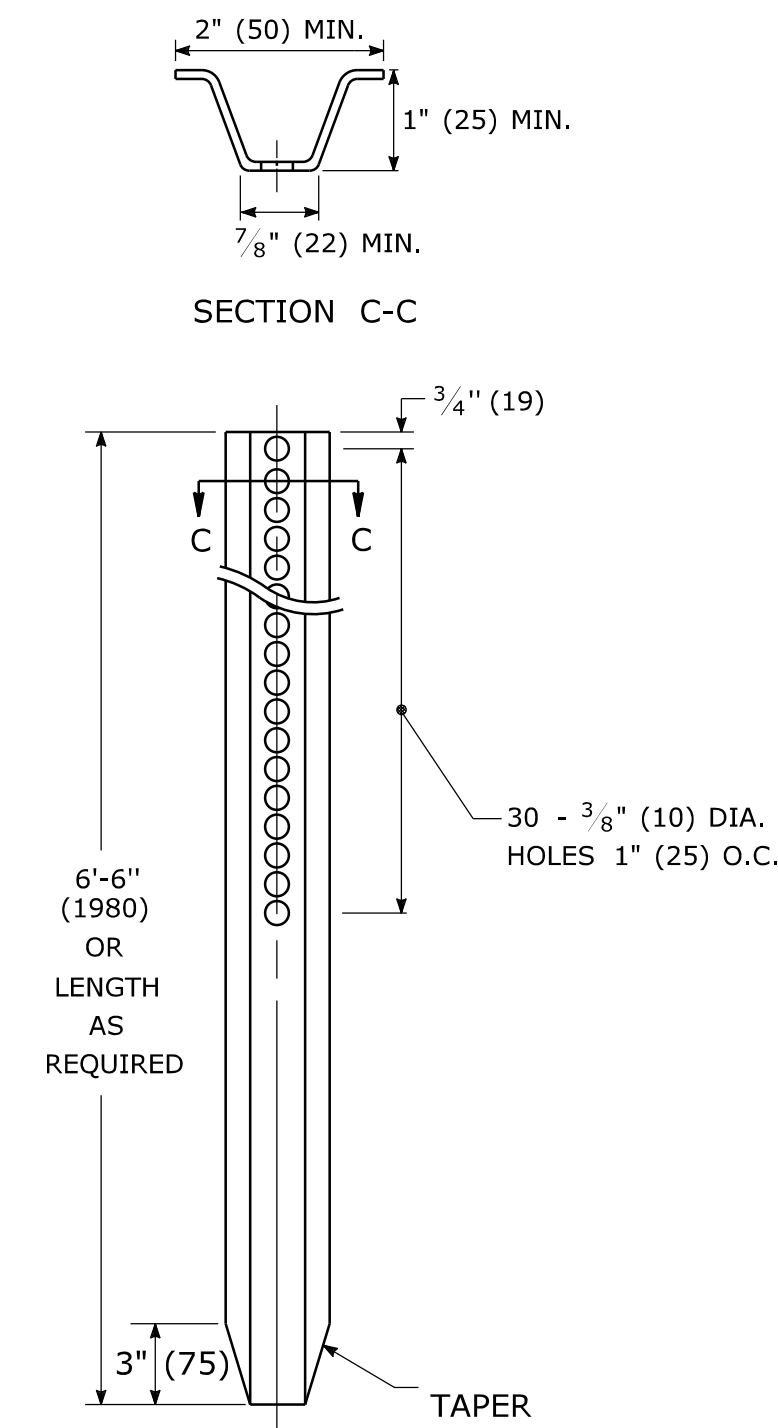


TYPICAL BACK-UP PLATE



METAL DELINEATOR POST

WT./FT. = 1.12 LBS. MIN.
 (MASS/m = 1.67 kg/m MIN.)

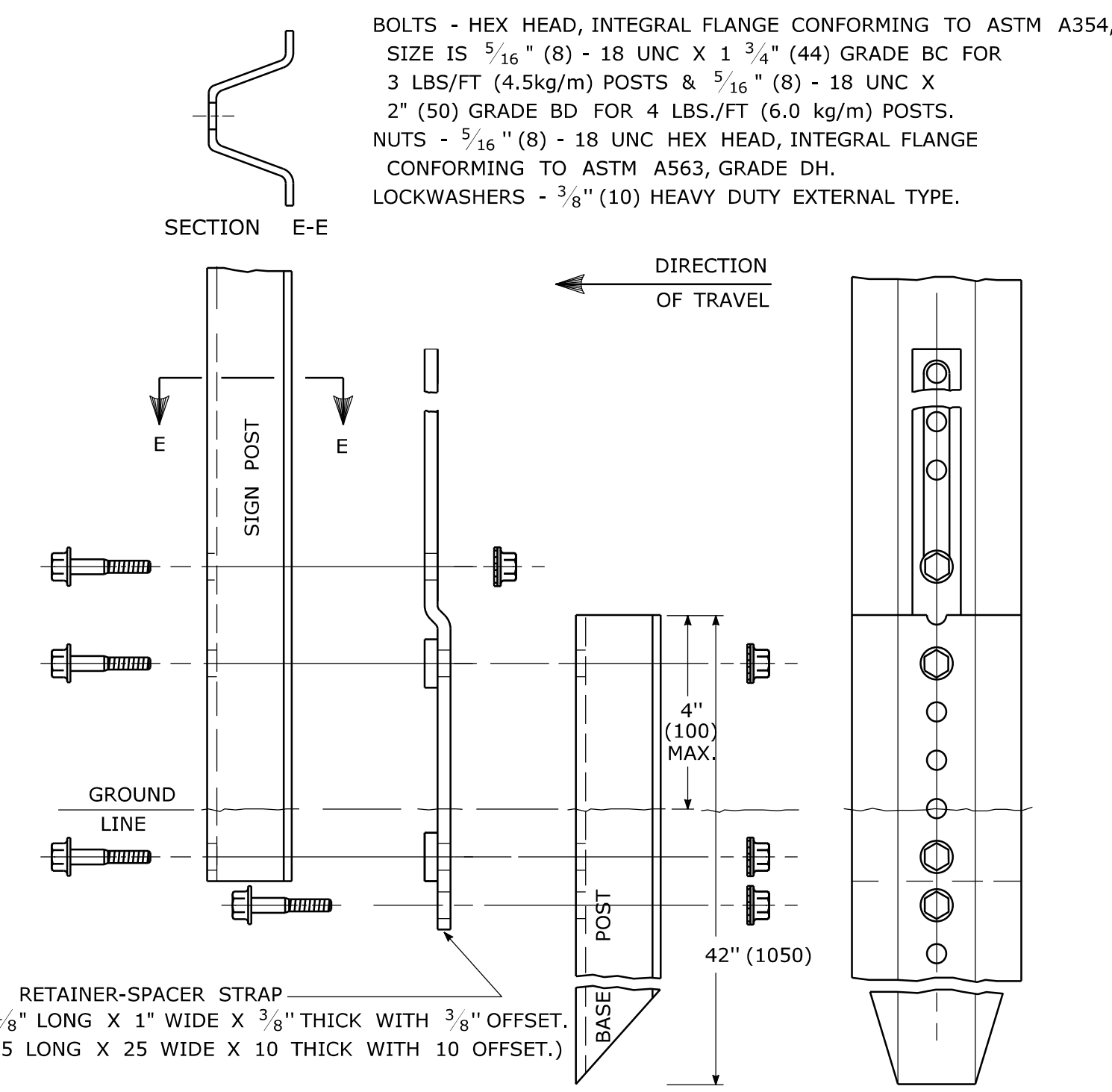


GENERAL NOTES:

- STEEL FOR DELINEATOR POSTS SHALL BE ASTM A36/A36(m) STEEL. STEEL FOR ALL OTHER POSTS SHALL CONFORM TO THE MECHANICAL REQUIREMENTS OF ASTM A 499 GRADE 60 AND TO THE CHEMICAL REQUIREMENTS OF ASTM A1 CARBON STEEL TEE RAIL HAVING NOMINAL WEIGHT (MASS) OF 91lbs. (45 kg.) OR GREATER PER LINEAR YARD (METER).
- AFTER FABRICATION, ALL STEEL POSTS, STRAPS AND PLATES SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A123/A123(m).
- WASHERS FOR BREAKAWAY INSTALLATIONS SHALL MEET ASTM F436, TYPE 1.
- ALL BOLTS, NUTS, AND WASHERS FOR BREAKAWAY INSTALLATIONS SHALL BE GALVANIZED TO MEET THE REQUIREMENTS OF ASTM A153/A153(m).
- ALL SIGN POSTS SHALL HAVE BREAKAWAY FEATURES THAT MEET ASHTO REQUIREMENTS CONTAINED IN THE CURRENT "STANDARD SPECIFICATIONS FOR STRUCTURAL SUPPORTS FOR HIGHWAY SIGNS, LUMINAIRES AND TRAFFIC SIGNALS." THE BREAKAWAY FEATURES SHALL BE STRUCTURALLY ADEQUATE TO CARRY THE SIGNS SHOWN IN THE PLANS AT 60 mph (97 km/h) WIND LOADINGS. INSTALLATIONS SHALL BE IN ACCORDANCE WITH MANUFACTURERS RECOMMENDATIONS.
- TYPE A POSTS - 3 lbs/ft (4.5 kg/m) TYPE B POSTS - 4 lbs/ft (6 kg/m).

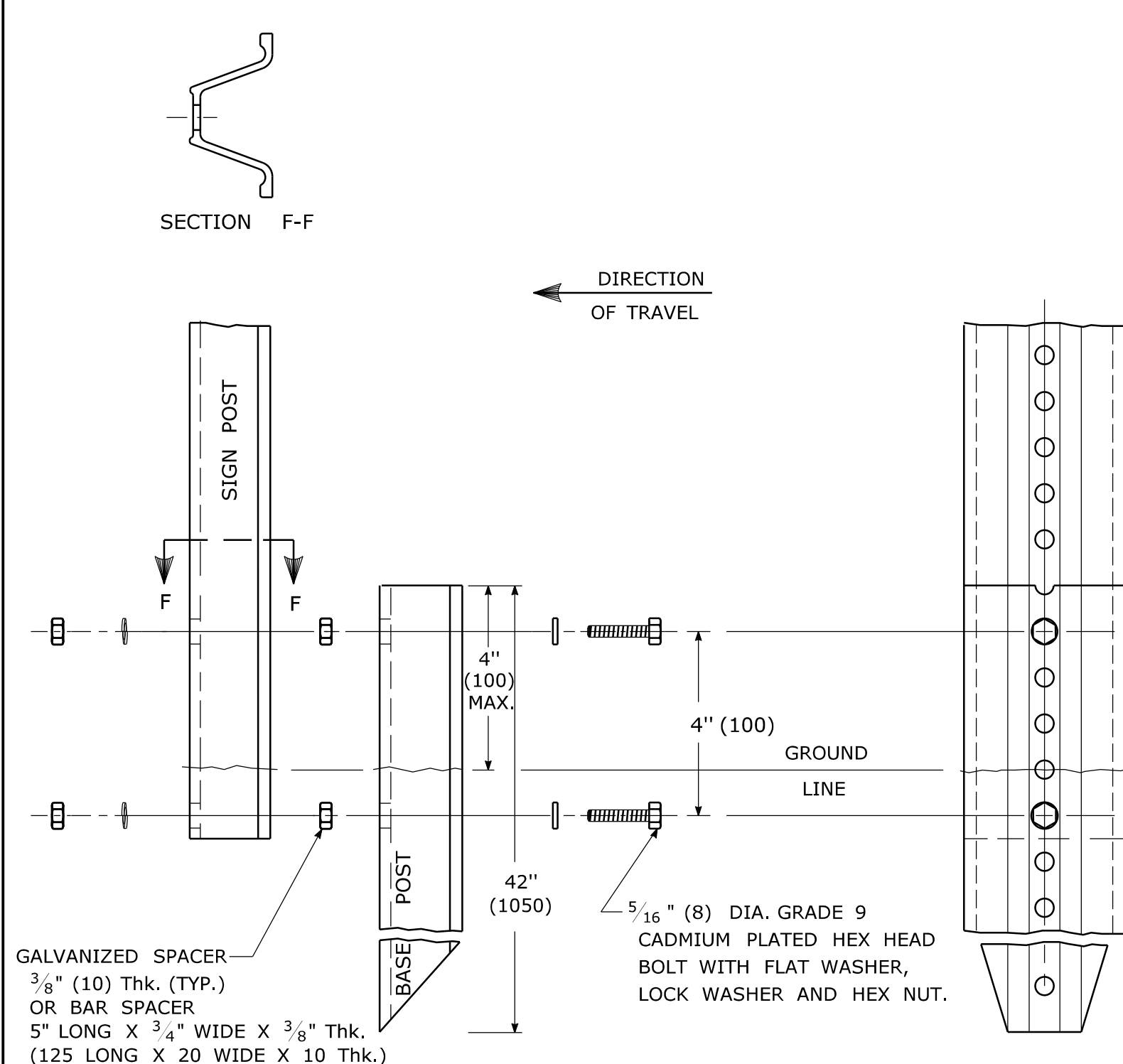
BREAKAWAY TYPE I INSTALLATION

FOR 3 & 4 LB. POSTS
 (FOR 4.5 & 6.0 kg/m POSTS)

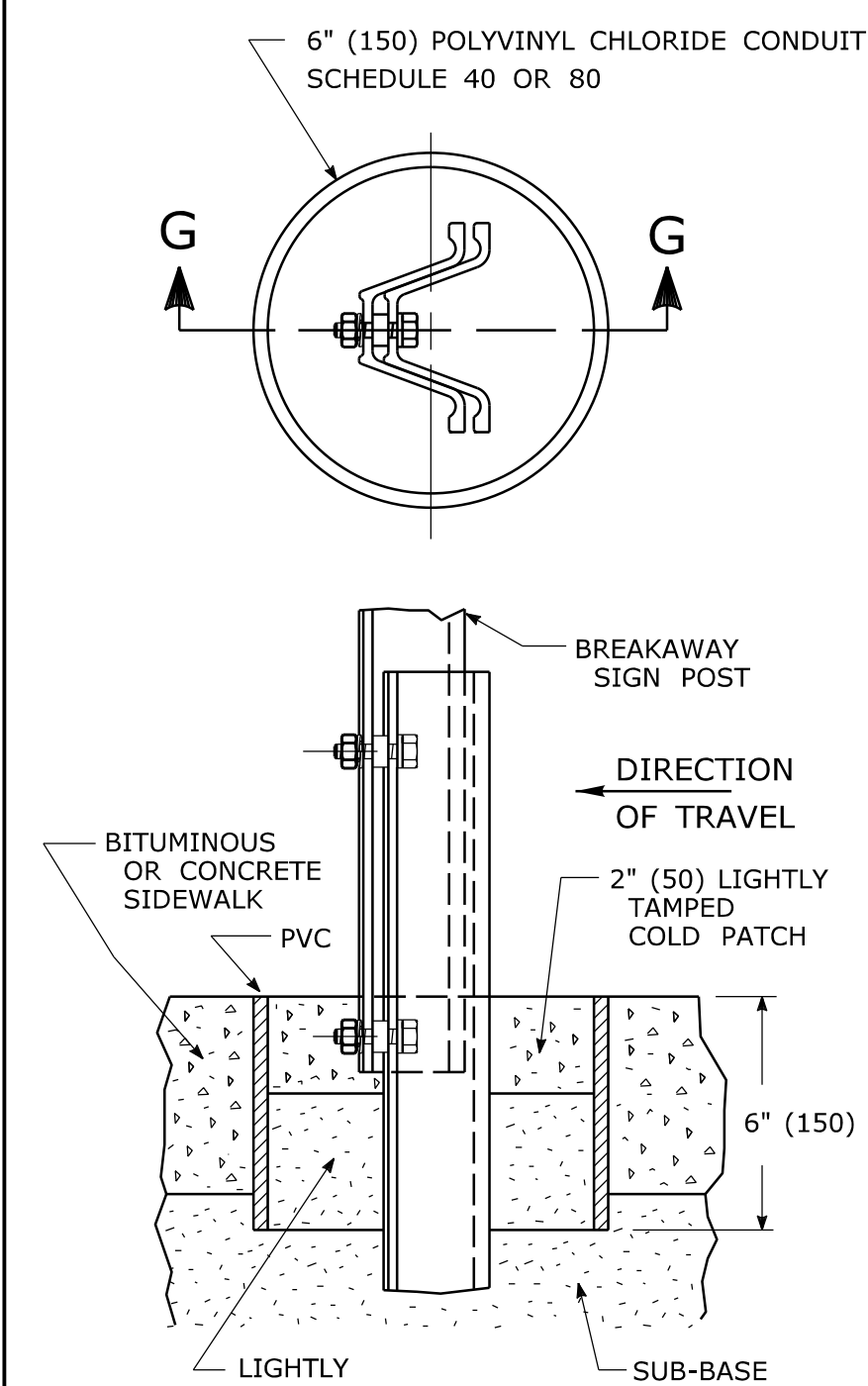


BREAKAWAY TYPE II INSTALLATION

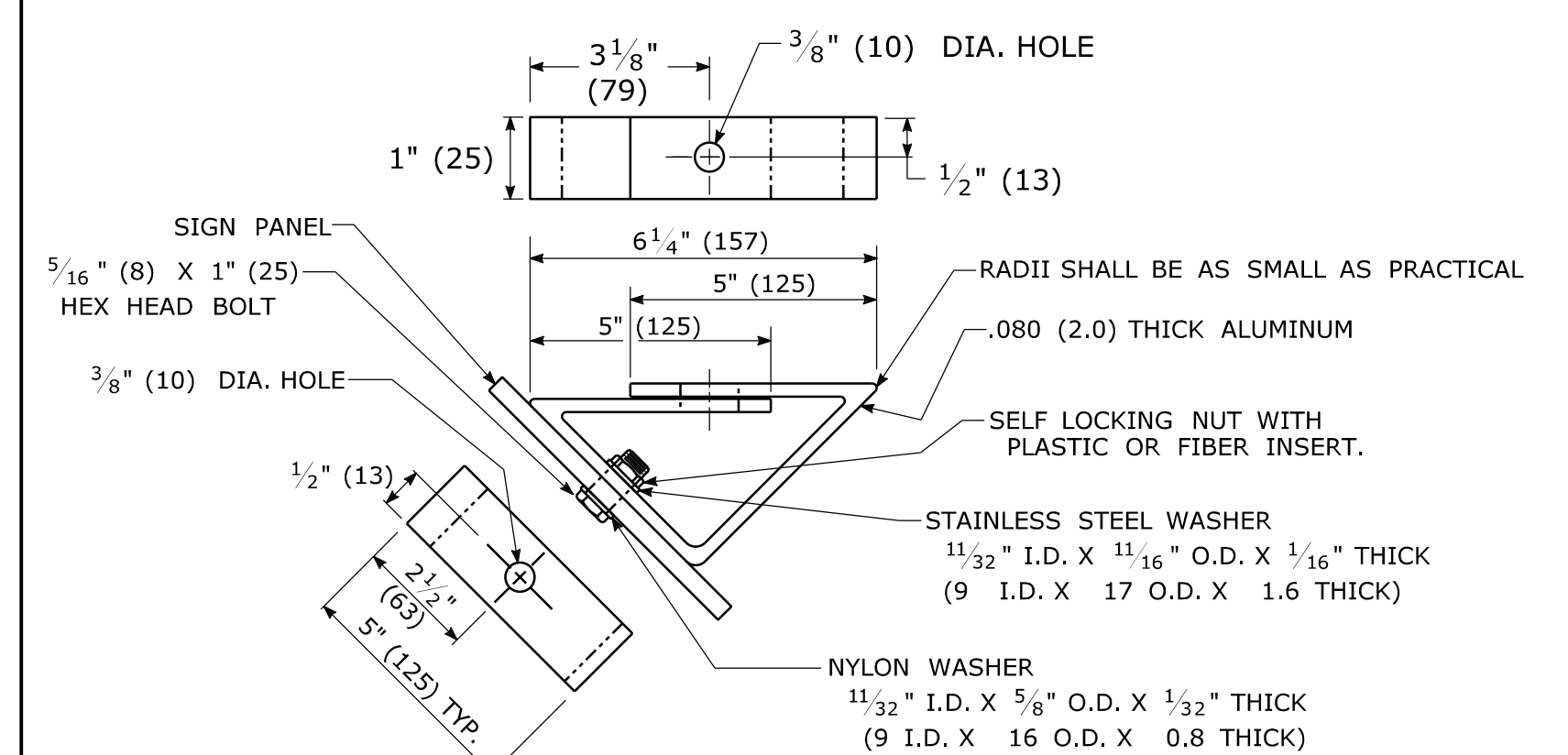
FOR 3 & 4 LB. POSTS
 (FOR 4.5 & 6.0 kg/m POSTS)



TYPICAL SLEEVE FOR PAVED AREAS



45° SUBMOUNTING BRACKET



REV.	DATE	MINOR REVISIONS	REVISION DESCRIPTION
1	2-2011	MINOR REVISIONS	

Plotted Date: 3/22/2011

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DIMENSIONS ARE IN ENGLISH ("") & METRIC UNITS (mm). METRIC DIMENSIONS ARE ROUNDED: - OVER 1" TO NEAREST 5 mm. - UNDER 1" TO NEAREST 1 mm.

STATE OF CONNECTICUT DEPARTMENT OF TRANSPORTATION

NOT TO SCALE

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1208_02

SUBMITTED BY: NAME/DATE/TIME:

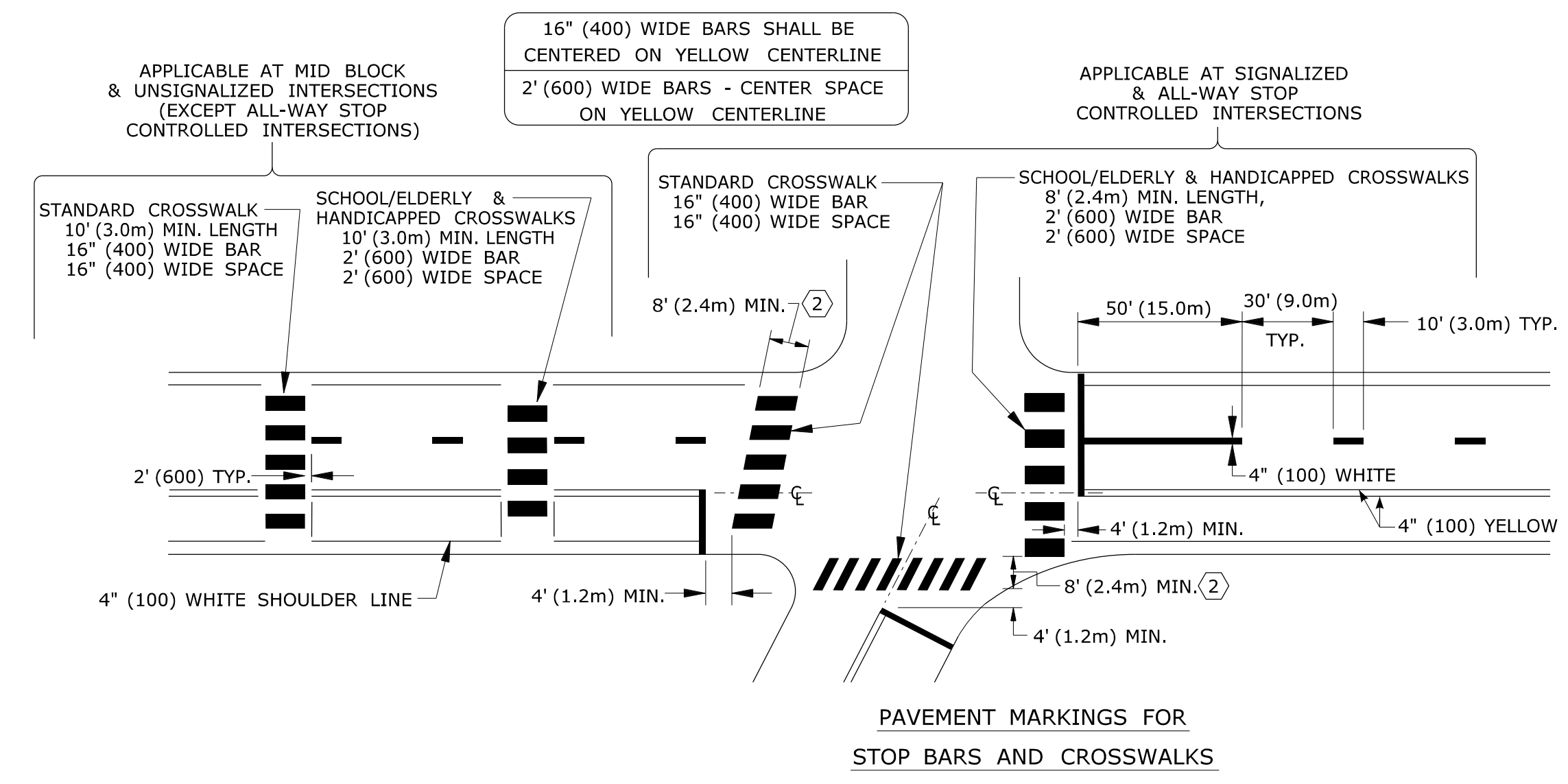
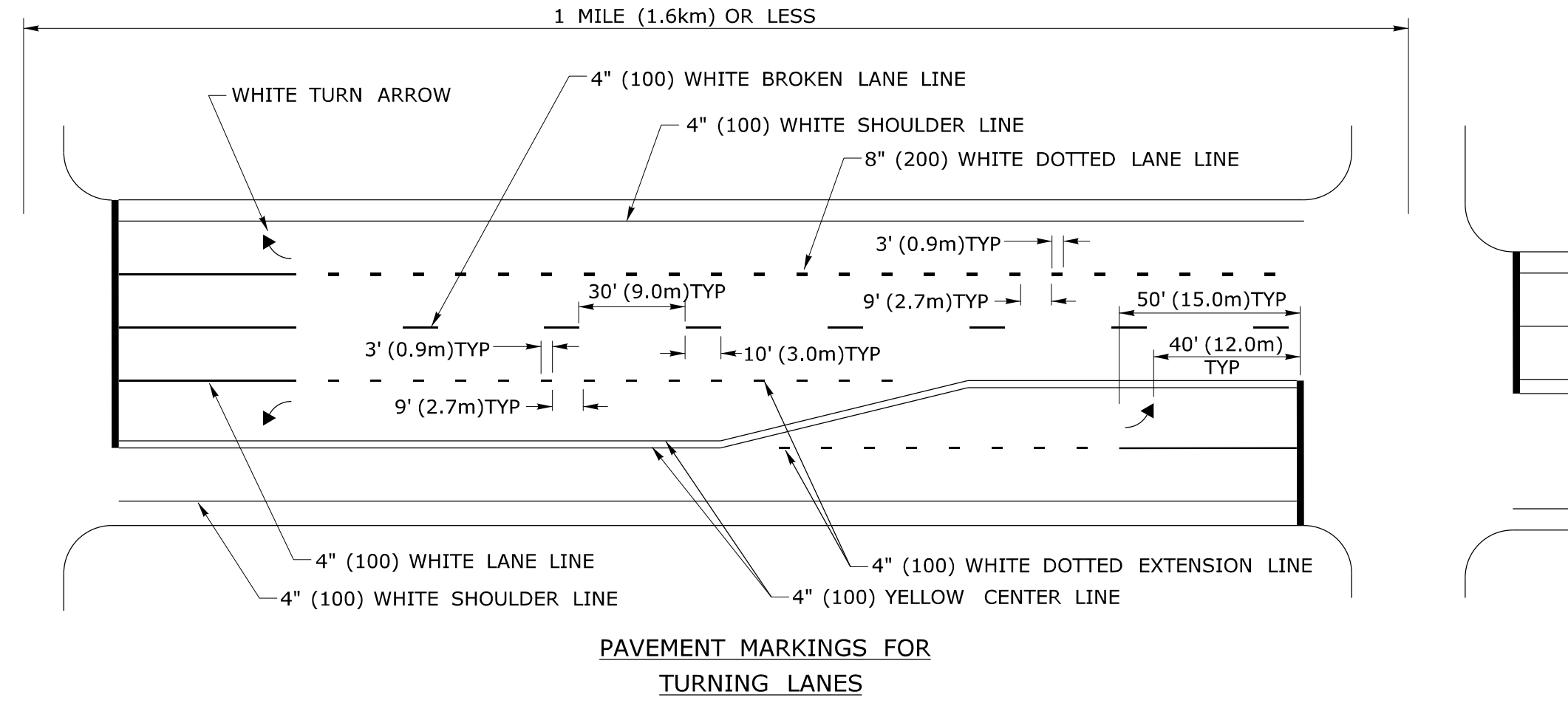
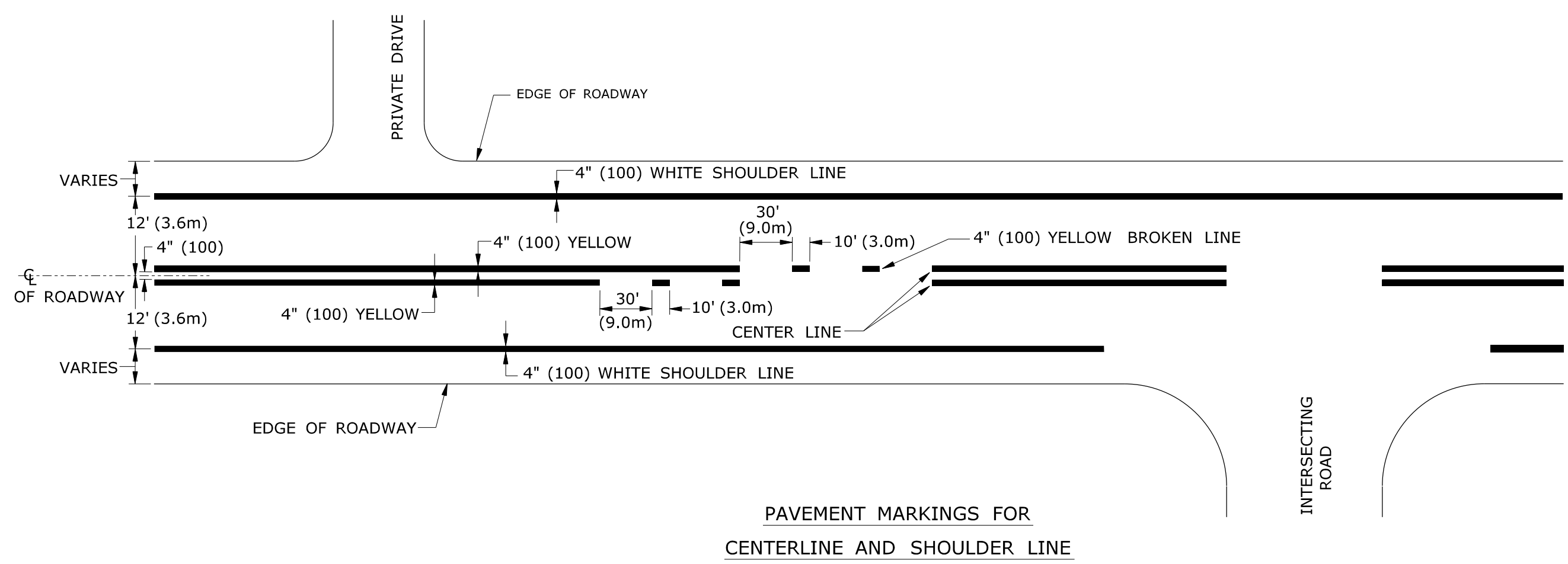
APPROVED BY: NAME/DATE/TIME:

CTDOT STANDARD SHEET

OFFICE OF ENGINEERING

STANDARD SHEET TITLE:
METAL SIGN POSTS AND SIGN MOUNTING DETAILS

STANDARD SHEET NO.:
TR-1208_02



- NOTES:**
- STOP-BARS**
- STOP BARS SHALL BE WHITE.
 - STOP BARS SHALL BE 12" (300) MIN. UNLESS OTHERWISE NOTED ON PLANS.
 - STOP BARS TO BE MARKED A MINIMUM OF 4' (1.2m) IN ADVANCE OF NEAREST EDGE OF CROSSWALK.
 - IN ABSENCE OF MARKED CROSSWALK THE STOP BAR SHALL BE PLACED AT THE DESIRED STOPPING POINT. NO MORE THAN 30' (9.0m) LESS THAN 5' (1.5m) FROM THE NEAREST EDGE OF THE INTERSECTING ROADWAY AND 90° TO THE CENTERLINE OF ROADWAY.
 - THE STOP BAR SHALL ORDINARILY BE PLACED IN LINE WITH THE STOP SIGN. HOWEVER, IF THE STOP SIGN CANNOT BE LOCATED EXACTLY WHERE VEHICLES ARE EXPECTED TO STOP, THE STOP BAR SHOULD BE PLACED AT THE STOPPING POINT.
 - STOP BARS AND CENTERLINE (WHEN SIDE STREET WIDTHS ARE 16' (4.8m) OR MORE) ARE TO BE MARKED ON SIDE STREETS WITHIN THE LIMITS OF CONSTRUCTION UNLESS, OTHERWISE INDICATED, OR AS DIRECTED BY THE ENGINEER.
- CROSSWALKS**
- CROSSWALK MARKINGS SHALL BE WHITE.
 - AT LOCATIONS WHERE THE CROSSWALK IS SKEWED, BARS TO BE PARALLEL TO ϕ AND ENDS OF BARS TO BE PARALLEL. THE LENGTH OF THE BARS WILL VARY DEPENDING ON THE ANGLE OF SKEW.
 - SCRAMBLE WALKS TO BE MARKED WITH ONE 24" WIDE LINE ACROSS EACH APPROACH.
 - BARS SHALL NORMALLY BE NO CLOSER THAN 2' FROM CURB LINE/EDGE OF ROAD. WHERE EXCESS SPACE MAY DEVELOP THIS DISTANCE MAY BE DECREASED TO 1'.
 - ONLY FULL LENGTH BARS ARE TO BE INSTALLED AT CORNERS.

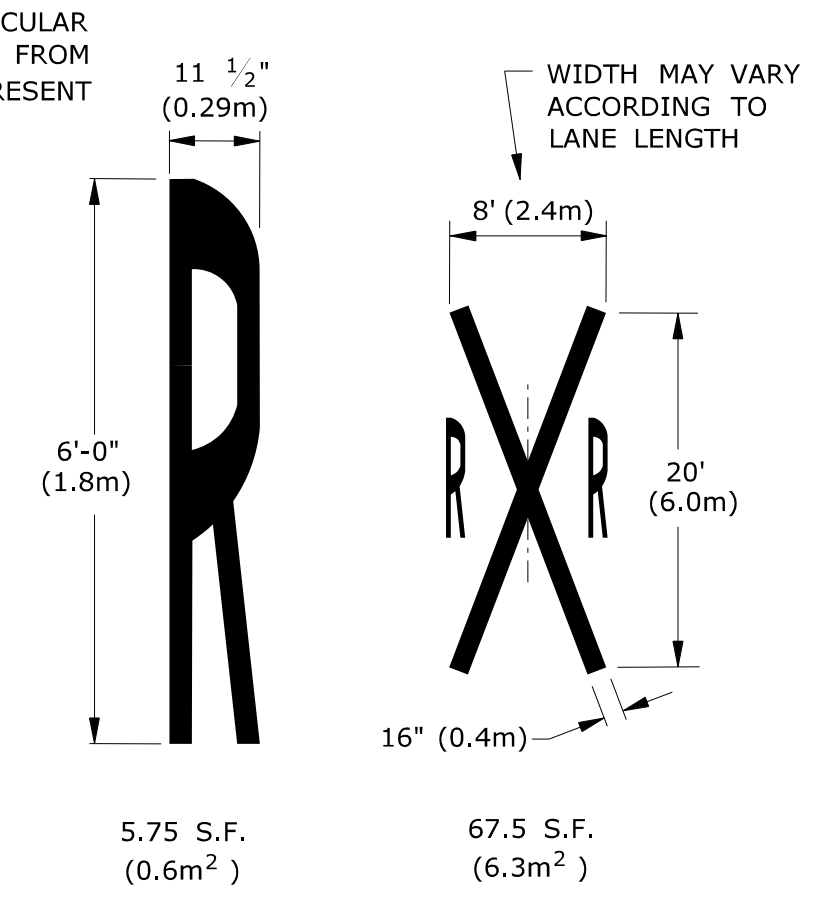
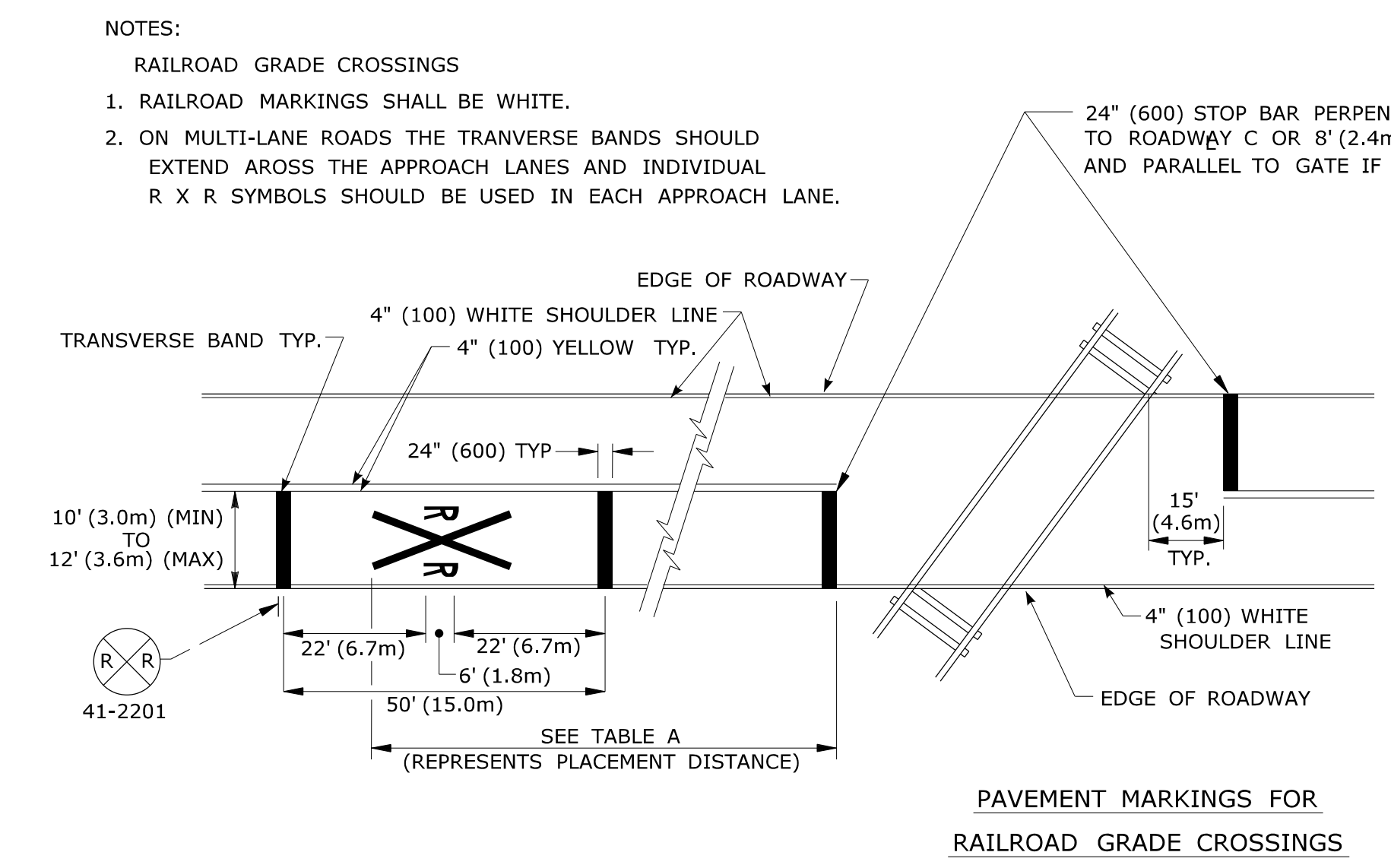
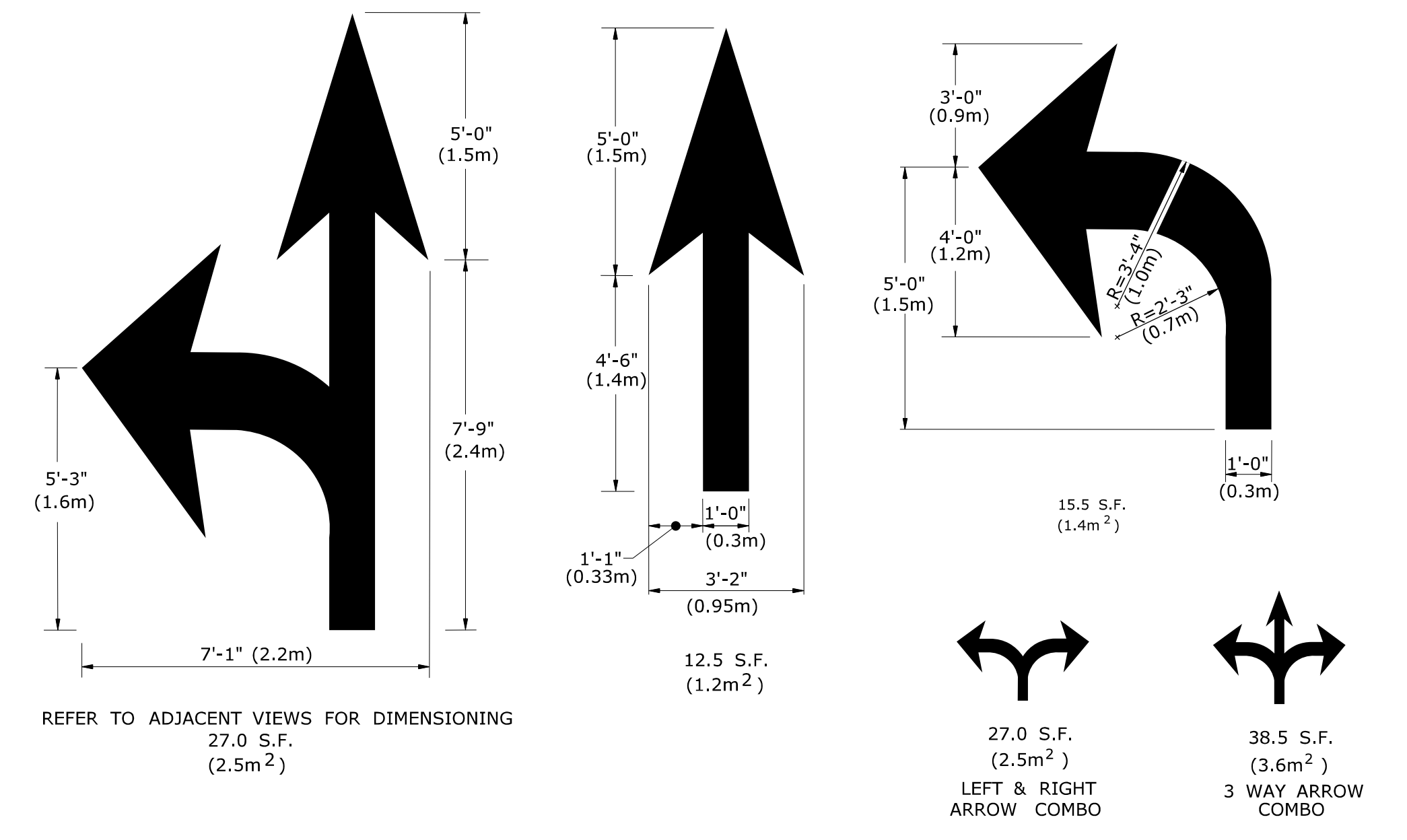
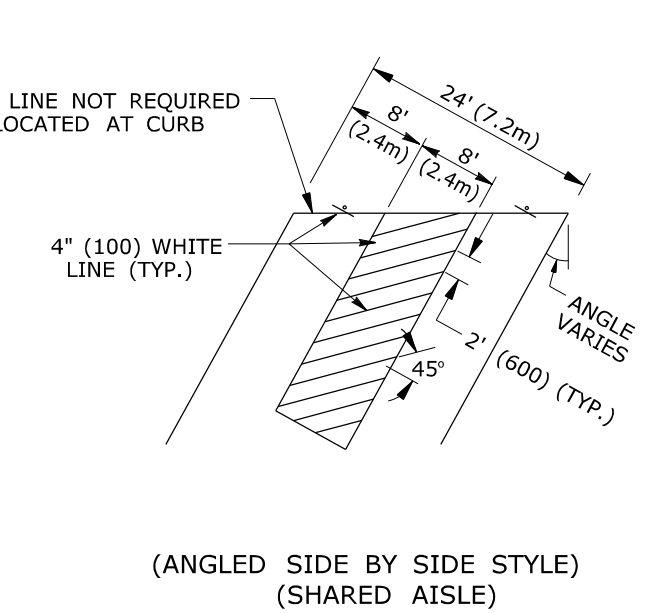
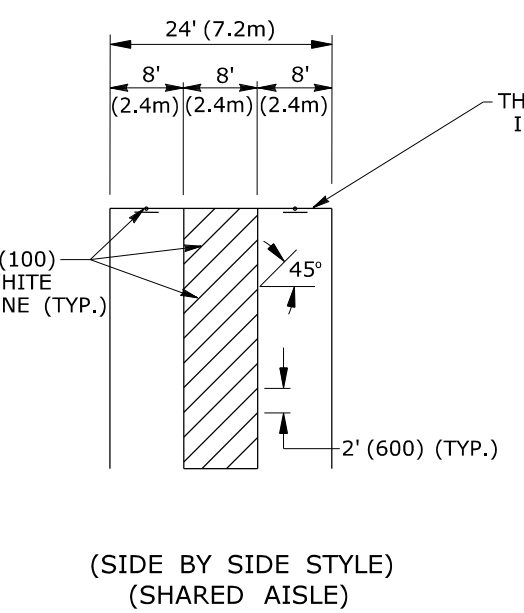
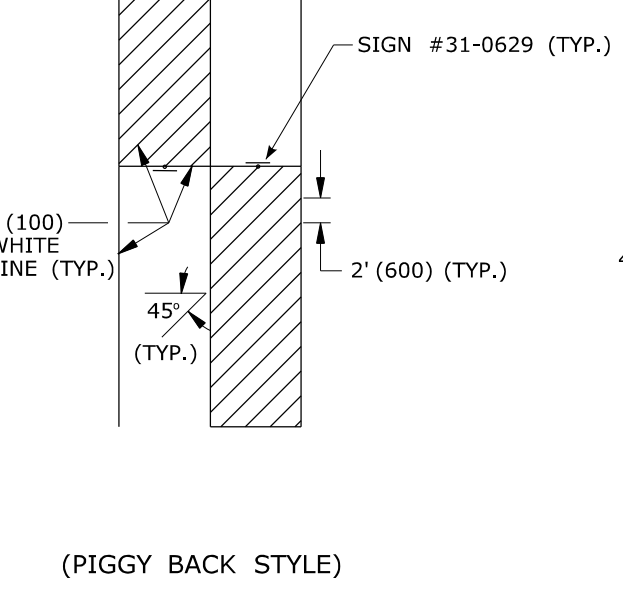
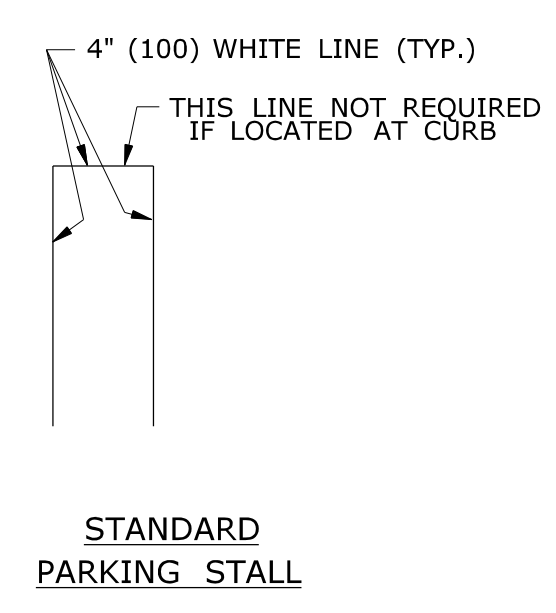


TABLE A	
POSTED OR 85 PERCENTILE SPEED M.P.H.	DISTANCE FT. (m)
20	*
25	*
30	100 (30)
35	150 (46)
40	225 (69)
45	300 (91)
50	375 (114)
55	450 (137)
60	550 (168)
65	650 (198)

* NO SUGGESTED MINIMUM DISTANCE, AT THESE SPEEDS, SIGN LOCATION DEPENDS ON PHYSICAL CONDITIONS AT SITE, HOWEVER SHOULD NOT BE LESS THAN 50' (15m).



REV.	DATE	ADDED PAVEMENT MARKINGS FOR TURNING LANES.	REVISION DESCRIPTION
1	2-2011	ADDED PAVEMENT MARKINGS FOR TURNING LANES.	

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NOT TO SCALE

STATE OF CONNECTICUT
DEPARTMENT OF TRANSPORTATION

Plotted Date: 2/16/2011

Filename: CTDOT_TRAFFIC_STD.dgn Model: TR-1210_03

SUBMITTED BY: _____ NAME/DATE/TIME: _____

APPROVED BY: _____ NAME/DATE/TIME: _____

CTDOT
STANDARD SHEET
OFFICE OF ENGINEERING

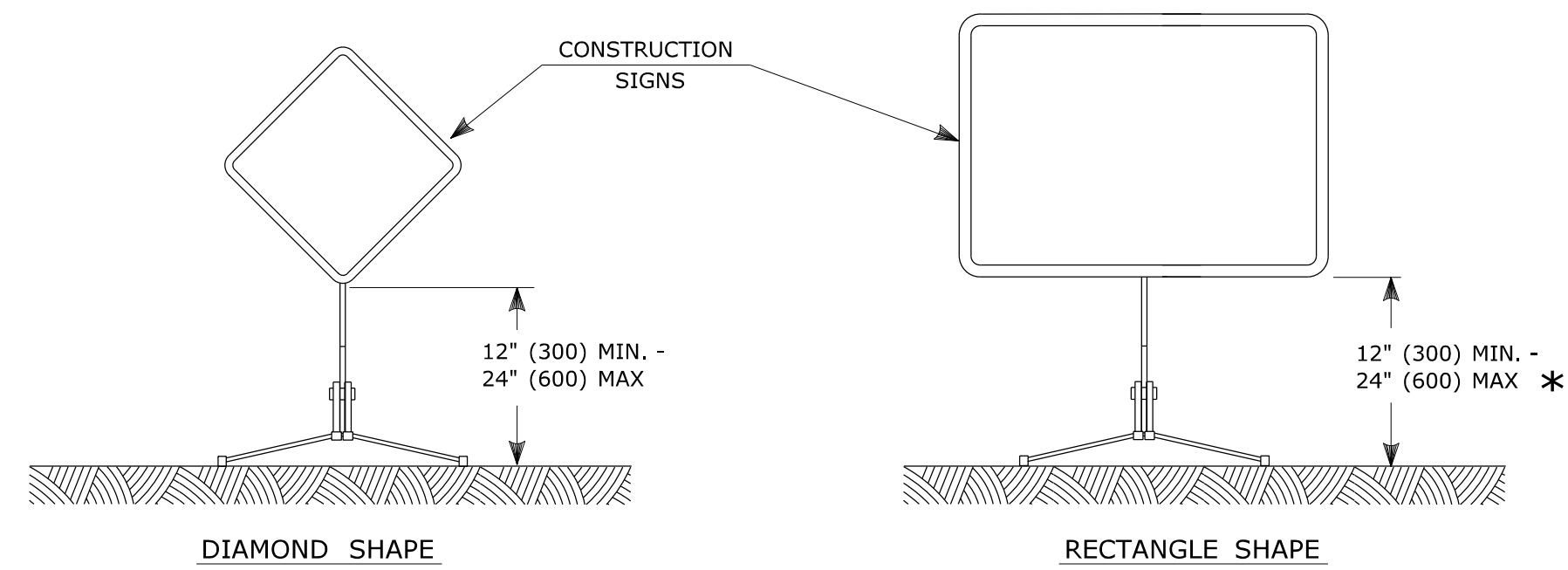
STANDARD SHEET TITLE:
SPECIAL DETAILS AND PAVEMENT MARKINGS FOR TWO-WAY HIGHWAYS

STANDARD SHEET NO.:
TR-1210_03

E5 - SERIES				G20 - SERIES				M4 - SERIES				R1 - SERIES				R9 & R11 - SERIES				W1 - SERIES				W3 - SERIES																																																																																																																															
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5.0	24X30	80-9403	1																																																																																																																																																				
7.5	30X36	80-9404	1																																																																																																																																																				
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25.0	60	80-9445R	2																																																																																																																																																				
<p>CONSTRUCTION AHEAD</p> <p>SIDEWALK USE RESTRICTED STATE LIABILITY LIMITED</p> <p>GENERAL STATUTES SEC 13a-115, 13a-145 COMMISSIONER OF TRANSPORTATION</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16-S</td><td>10.0</td><td>48X30</td><td>80-1619</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16-S	10.0	48X30	80-1619	2	<p>BUSINESS ACCESS</p> <p>VARIABLE ARROW COPY & BORDER - WHITE BACKGROUND - BLUE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>50-5934</td><td>1</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	50-5934	1	<p>DETOUR</p> <p>VARIABLE ARROW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>5.0</td><td>30X24</td><td>80-9710</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.0	30X24	80-9710	2	<p>R4 - SERIES</p> <p>R4-7</p> <p>COPY & BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>5.0</td><td>24X30</td><td>31-1526</td><td>1</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	5.0	24X30	31-1526	1	<p>ROAD CLOSED</p> <p>COPY & BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>10.0</td><td>48X30</td><td>80-9080</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	10.0	48X30	80-9080	2	<p>ROAD WORK AHEAD FINES DOUBLED</p> <p>COPY & BORDER - BLACK BACKGROUND - WHITE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>14.0</td><td>48X42</td><td>31-1906</td><td>2</td></tr> <tr><td>22.5</td><td>60X54</td><td>31-1907</td><td>2</td></tr> <tr><td>30.0</td><td>72X60</td><td>31-1908</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	14.0	48X42	31-1906	2	22.5	60X54	31-1907	2	30.0	72X60	31-1908	2	<p>W1-8</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>25.0</td><td>60</td><td>80-9444L</td><td>2</td></tr> <tr><td>25.0</td><td>60</td><td>80-9446R</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	25.0	60	80-9444L	2	25.0	60	80-9446R	2	<p>W3-3</p> <p>TOP CIRCLE - RED MIDDLE CIRCLE - YELLOW BOTTOM CIRCLE - GREEN COPY & BORDER - BLACK BACKGROUND - ORANGE</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9052</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9053</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9052	1	16.0	48	80-9053	2																																							
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<p>W4-2</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9918L</td><td>2</td></tr> <tr><td>16.0</td><td>48</td><td>80-9917R</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9918L	2	16.0	48	80-9917R	2	<p>BUMP</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9901</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9902</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9901	1	16.0	48	80-9902	2	<p>00 M.P.H.</p> <p>SUBPLATE VARIABLE SPEED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>2.25</td><td>18</td><td>80-9568</td><td>1</td></tr> <tr><td>4.0</td><td>24</td><td>80-9569</td><td>1</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	2.25	18	80-9568	1	4.0	24	80-9569	1	<p>ROAD WORK AHEAD</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>6.25</td><td>30</td><td>80-9602</td><td>1</td></tr> <tr><td>9.0</td><td>36</td><td>80-9603</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9604</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	6.25	30	80-9602	1	9.0	36	80-9603	1	16.0	48	80-9604	2	<p>LEFT LANES CLOSED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9836</td><td>2</td></tr> <tr><td>16.0</td><td>48</td><td>80-9839</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9836	2	16.0	48	80-9839	2	<p>RIGHT LANES CLOSED</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>16.0</td><td>48</td><td>80-9803</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9804</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	16.0	48	80-9803	1	16.0	48	80-9804	2	<p>SURVEY CREW</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9607</td><td>1</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9607	1	<p>BLASTING ZONE 1000 FT</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>9.0</td><td>36</td><td>80-9620</td><td>1</td></tr> <tr><td>16.0</td><td>48</td><td>80-9625</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	9.0	36	80-9620	1	16.0	48	80-9625	2	<p>W3-2b</p> <table border="1"> <tr><th>AREA (SQ. FT)</th><th>SIZE (INCHES)</th><th>CONN. D.O.T. #</th><th>POSTS</th></tr> <tr><td>4.17</td><td>60X10</td><td>80-9913</td><td>2</td></tr> <tr><td>12.0</td><td>96X18</td><td>80-9914</td><td>2</td></tr> <tr><td>3.33</td><td>48X10</td><td>80-9916</td><td>2</td></tr> </table>				AREA (SQ. FT)	SIZE (INCHES)	CONN. D.O.T. #	POSTS	4.17	60X10	80-9913	2	12.0	96X18	80-9914	2	3.33	48X10	80-9916	2
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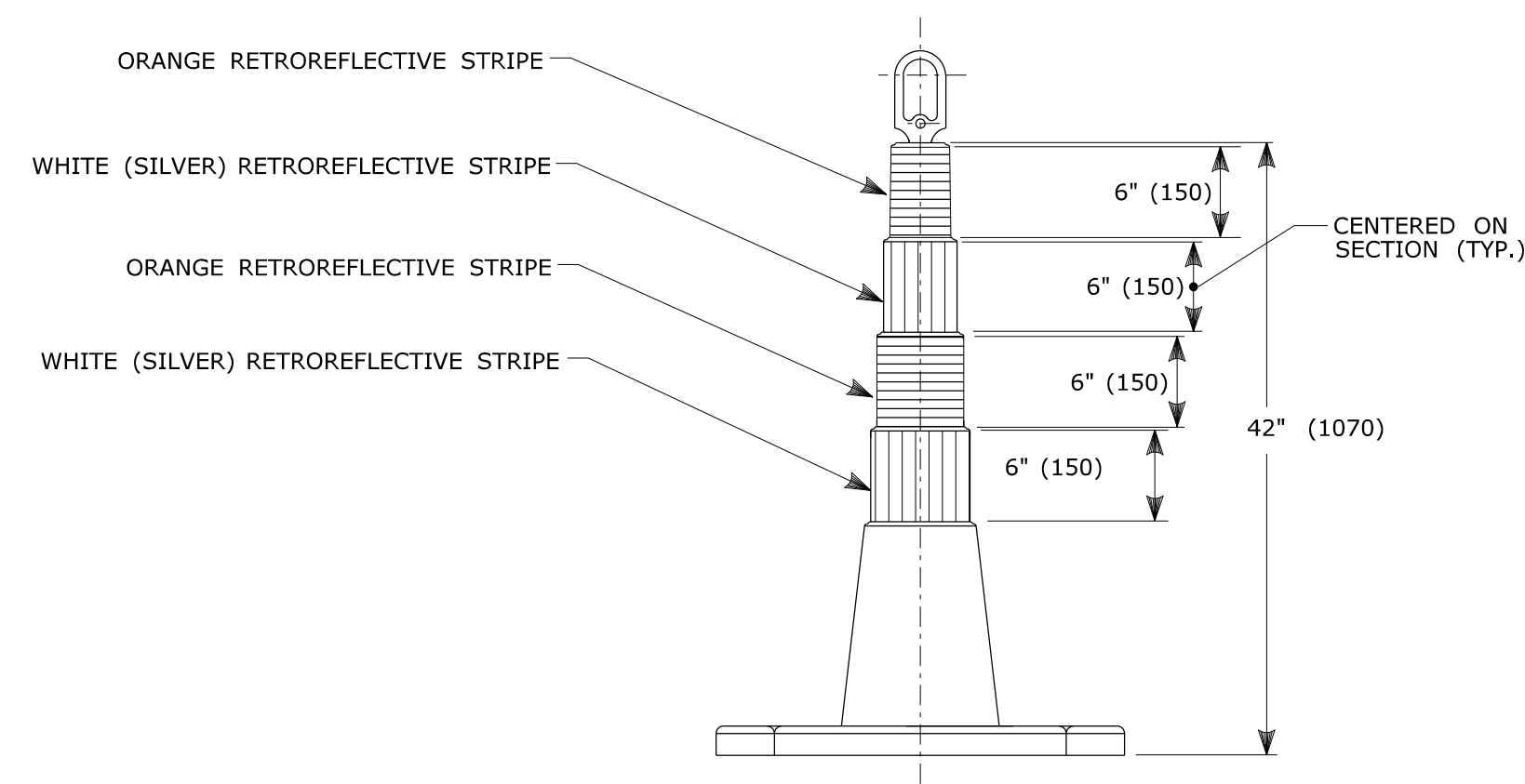
W4-W6 - SERIES				W8-W9 - SERIES				W13 - SERIES				W20 - SERIES				W21 - SERIES				W22 - SERIES				W3-2c																																																																																																																											
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CONSTRUCTION SIGNS

NOTES FOR PORTABLE SIGN SUPPORTS:

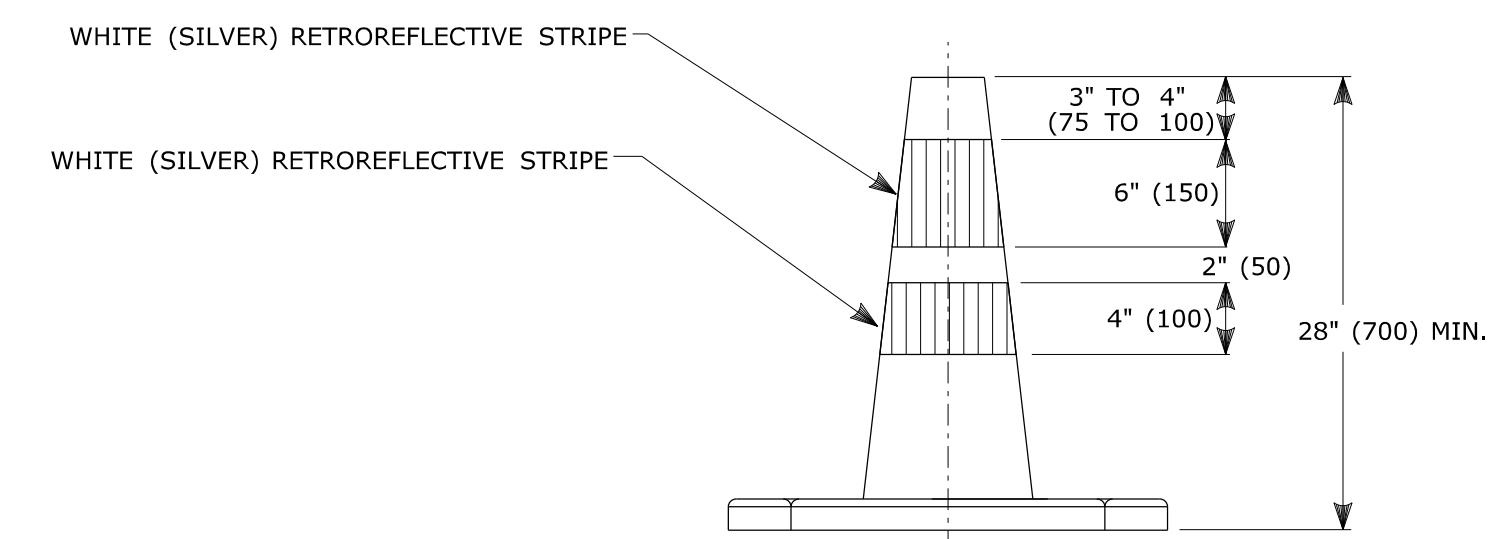
- SIGNS AND THEIR PORTABLE SUPPORTS SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
 - MOUNTING HEIGHT OF SIGNS SHALL BE A MINIMUM OF 12" (300) AND A MAXIMUM OF 24" (600). SIGNS SHALL BE MOUNTED HIGHER AS NEEDED TO MEET FIELD CONDITIONS OR AS DIRECTED BY THE ENGINEER.
 - THE ENGINEER RESERVES THE RIGHT TO REJECT ANY SUPPORT DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
 - PORTABLE SIGN SUPPORTS SHALL BE STABILIZED IN A MANNER THAT WILL NOT AFFECT THEIR COMPLIANCE WITH NCHRP REPORT 350 (TL-3).
- * FOR EXIT SIGNS, USE MIN. 72" (1800).



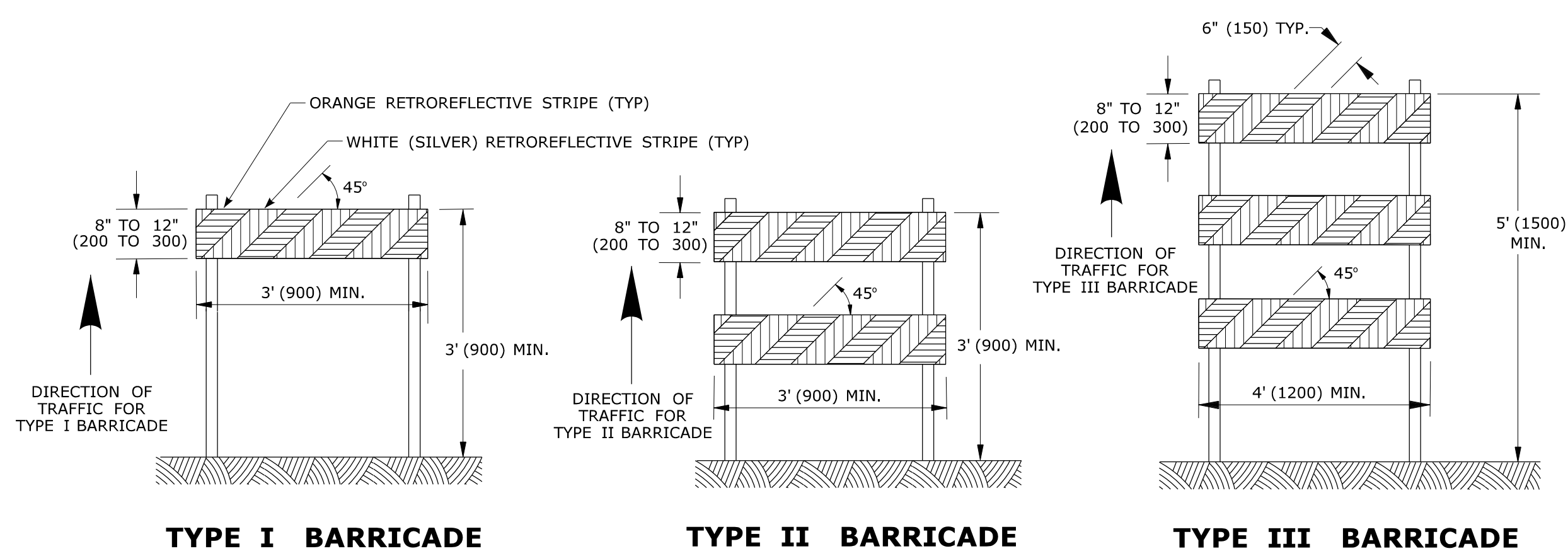
42" (1m) TRAFFIC CONE

NOTES:

- TRAFFIC CONES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
- IF RUBBER CONES ARE USED, THEY SHALL HAVE INTERIOR RIBS FOR RIGIDITY.
- IF PLASTIC CONES ARE USED, THEY SHALL BE COLOR IMPREGNATED.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY CONE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.



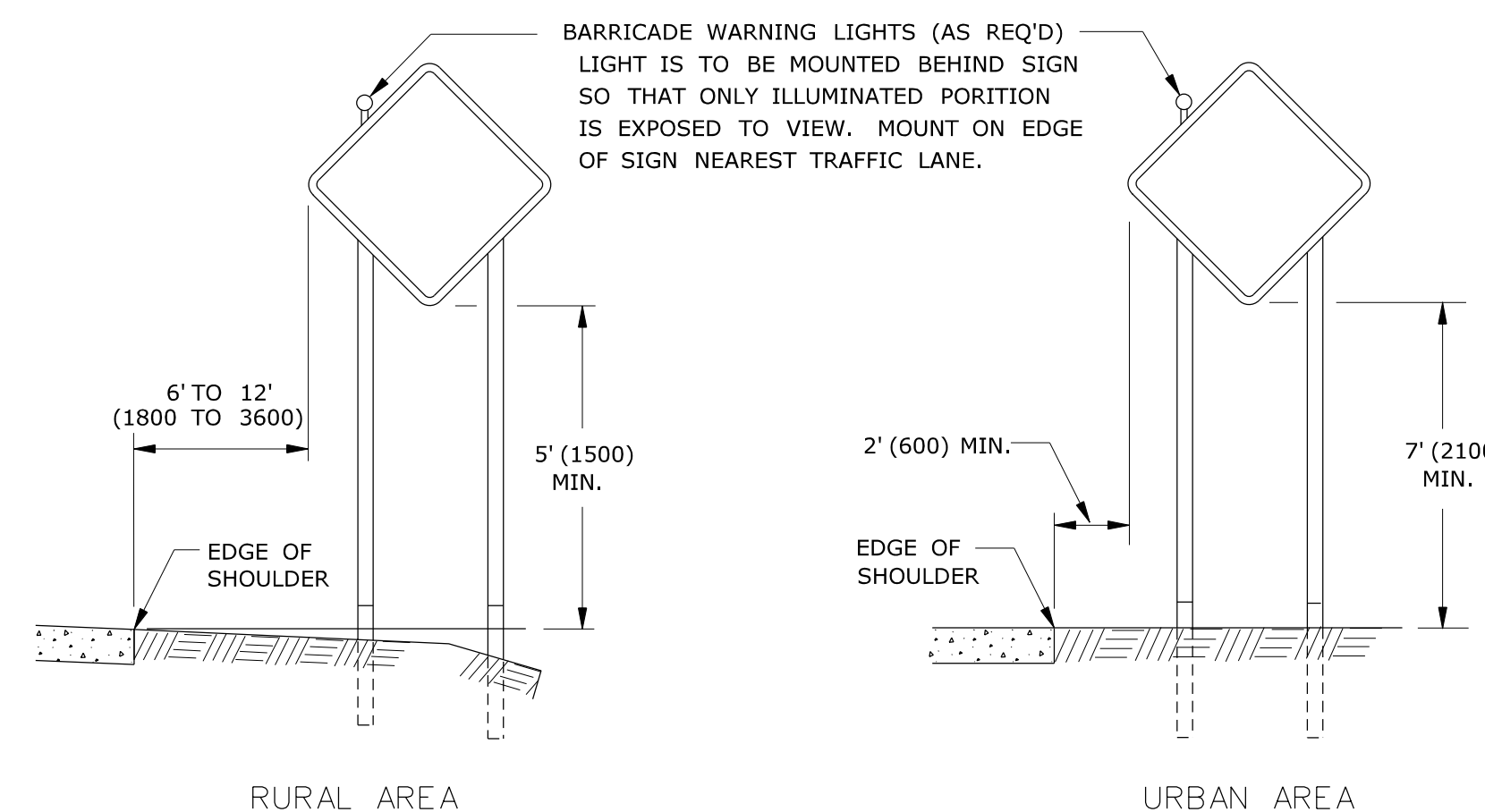
TRAFFIC CONE



CONSTRUCTION BARRICADES

NOTES:

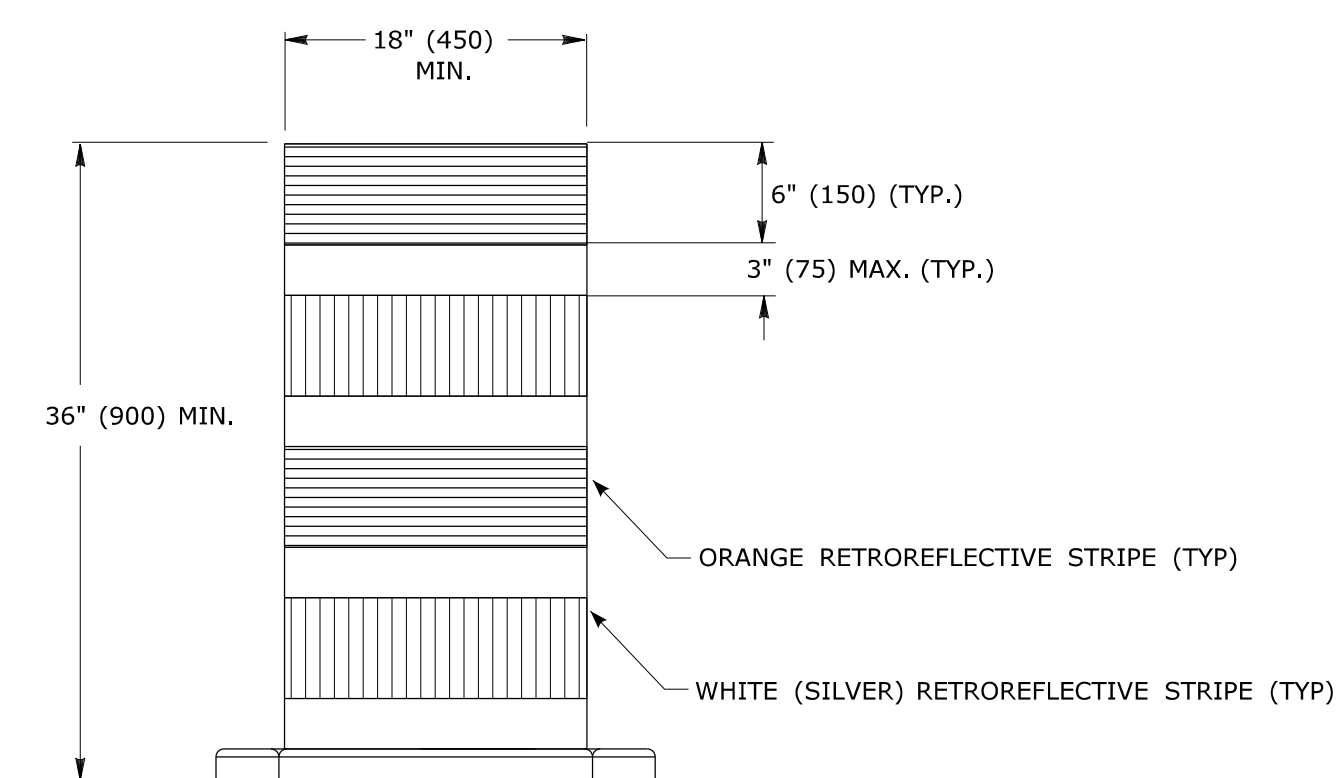
- CONSTRUCTION BARRICADES SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
- MARKINGS FOR BARRICADE RAILS SHALL BE ALTERNATE ORANGE AND WHITE STRIPES SLOPING DOWNWARD IN THE DIRECTION TRAFFIC IS TO PASS. 6" (150) WIDE STRIPES SHALL BE USED.
- THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS. RAILS FOR TYPE I AND TYPE II BARRICADES SHALL BE RETROREFLECTIVE ON BOTH SIDES. WHERE TRAFFIC PASSES ONLY IN ONE DIRECTION OF TRAVEL, ONLY THE SIDE FACING TRAFFIC SHALL BE RETROREFLECTIVE.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY BARRICADE DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- CORNERS OF BARRICADE RAILS SHALL BE ROUNDED.
- SIGNS MAY ONLY BE INSTALLED ON TYPE III BARRICADES AND SHALL BE PLACED SO AS TO COVER NO MORE THAN ONE BARRICADE RAIL.



**PLACEMENT OF CONSTRUCTION SIGNS
TYPICAL LONG TERM INSTALLATION**

NOTES:

- SUPPORTS SHALL BE METAL SIGN POSTS AND HAVE BREAK-AWAY FEATURES.
- SEE TYPICAL SHEETS:
 TYPICAL SIGN SUPPORT AND SIGN PLACEMENT DETAILS-GORE EXIT SIGN
 TYPICAL METAL SIGN POSTS AND SIGN MOUNTING DETAILS



**TRAFFIC DRUM
FRONT VIEW**

NOTES:

- TRAFFIC DRUM SHALL CONFORM TO THE REQUIREMENTS OF NCHRP REPORT 350 (TL-3) AND THE LATEST EDITION OF THE MUTCD.
- THE ENGINEER RESERVES THE RIGHT TO REJECT ANY DRUM DEEMED UNSUITABLE FOR THE PURPOSE INTENDED.
- THE ENTIRE AREA OF ORANGE AND WHITE STRIPES SHALL BE RETROREFLECTIVE SHEETING AS REQUIRED IN THE SPECIFICATIONS.
- THE SECTIONS OF DRUMS NOT COVERED WITH RETROREFLECTIVE STRIPES SHALL BE ORANGE.